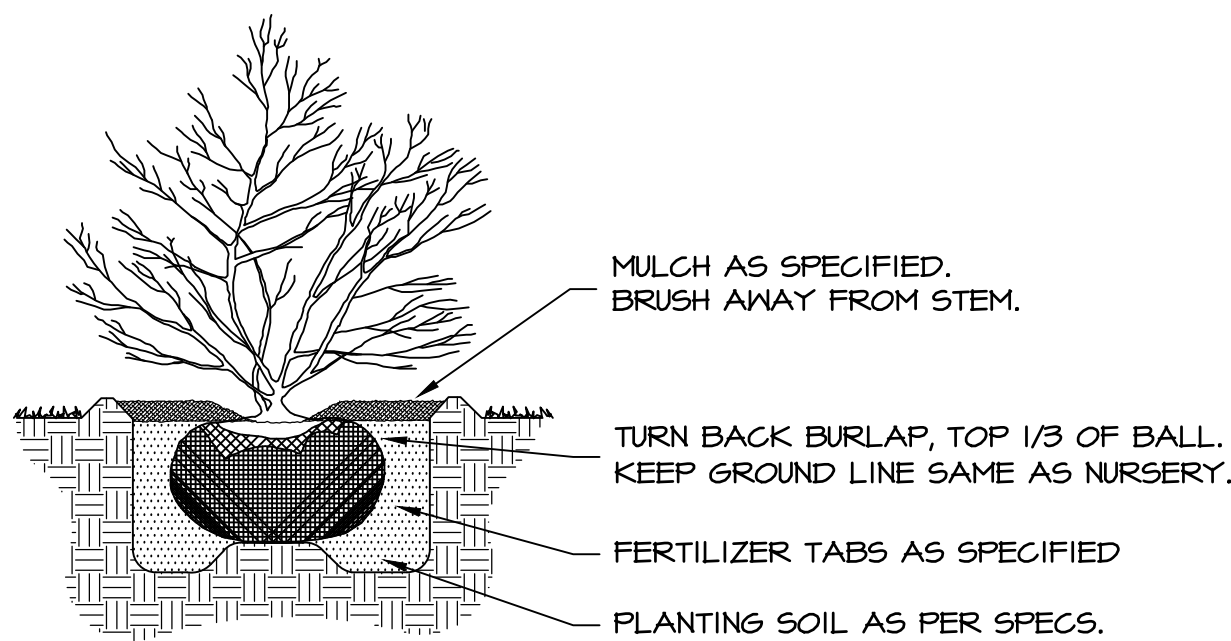


- NOTES:
1. REMOVE ALL TWINE, ROPE, OR BINDINGS FROM ALL TRUNKS.
  2. REMOVE BURLAP AND WIRE BASKETS FROM THE TOP 1/3 OF ALL ROOT BALLS AFTER PLANTING. IF SYNTHETIC WRAP/BURLAP IS USED, IT MUST BE COMPLETELY REMOVED.
  3. STAKING OF TREES TO BE THE CONTRACTOR'S OPTION; HOWEVER, THE CONTRACTOR IS RESPONSIBLE TO INSURE THAT ALL TREES ARE PLANTED STRAIGHT AND REMAIN STRAIGHT FOR A MIN OF 1 YEAR. ALL STAKING SHALL BE REMOVED AT THE END OF THE 1 YEAR WARRANTY PERIOD.
  4. TREES PLANTED IN TURF AREAS: REMOVE TURF 3' DIA. FROM TREE TRUNK.

### 1 TREE PLANTING/STAKING

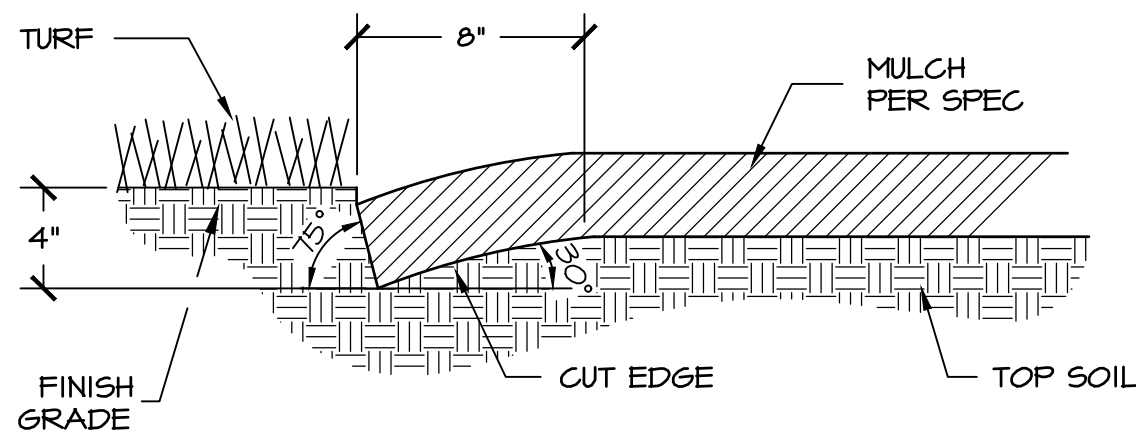
NOT TO SCALE



NOTE: DIG HOLE TWICE THE SIZE OF ROOTBALL.

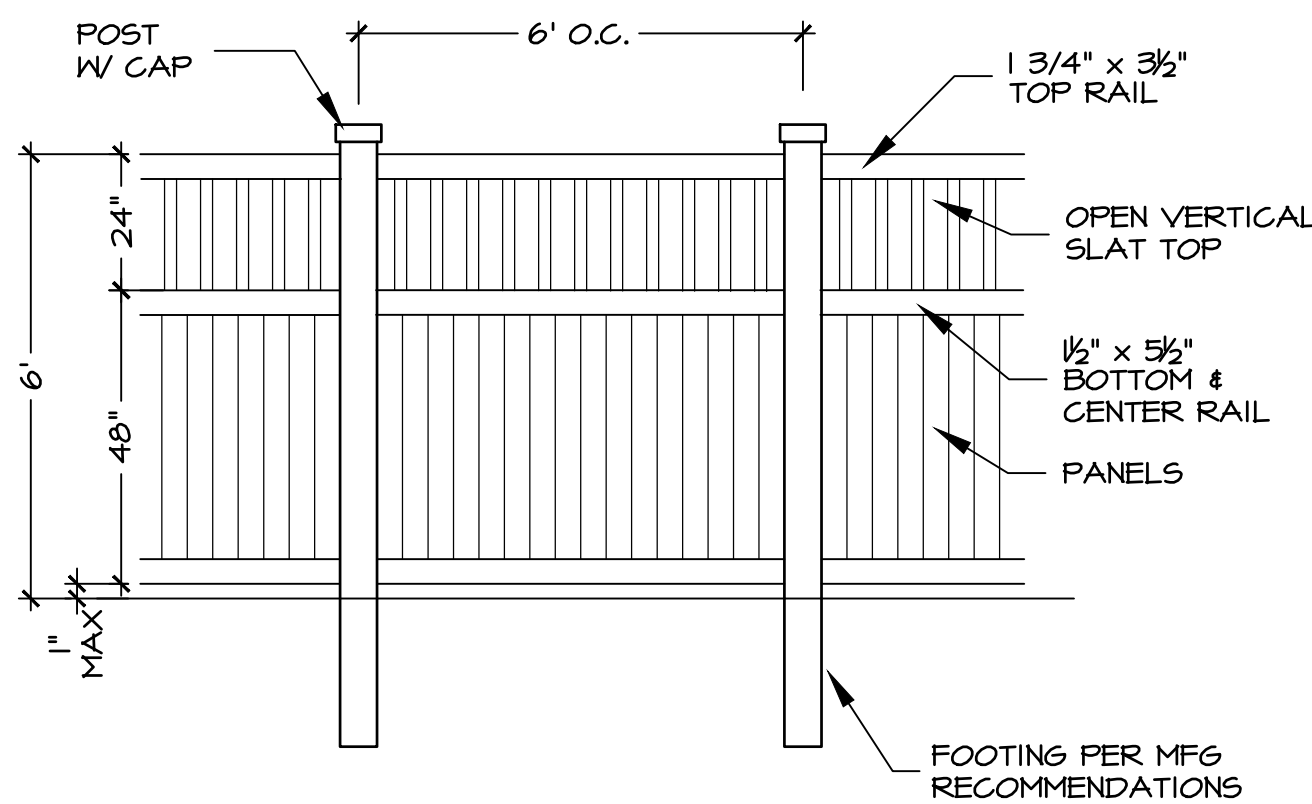
### 2 SHRUB PLANTING

NOT TO SCALE



### 3 PLANTER CUT BED EDGE

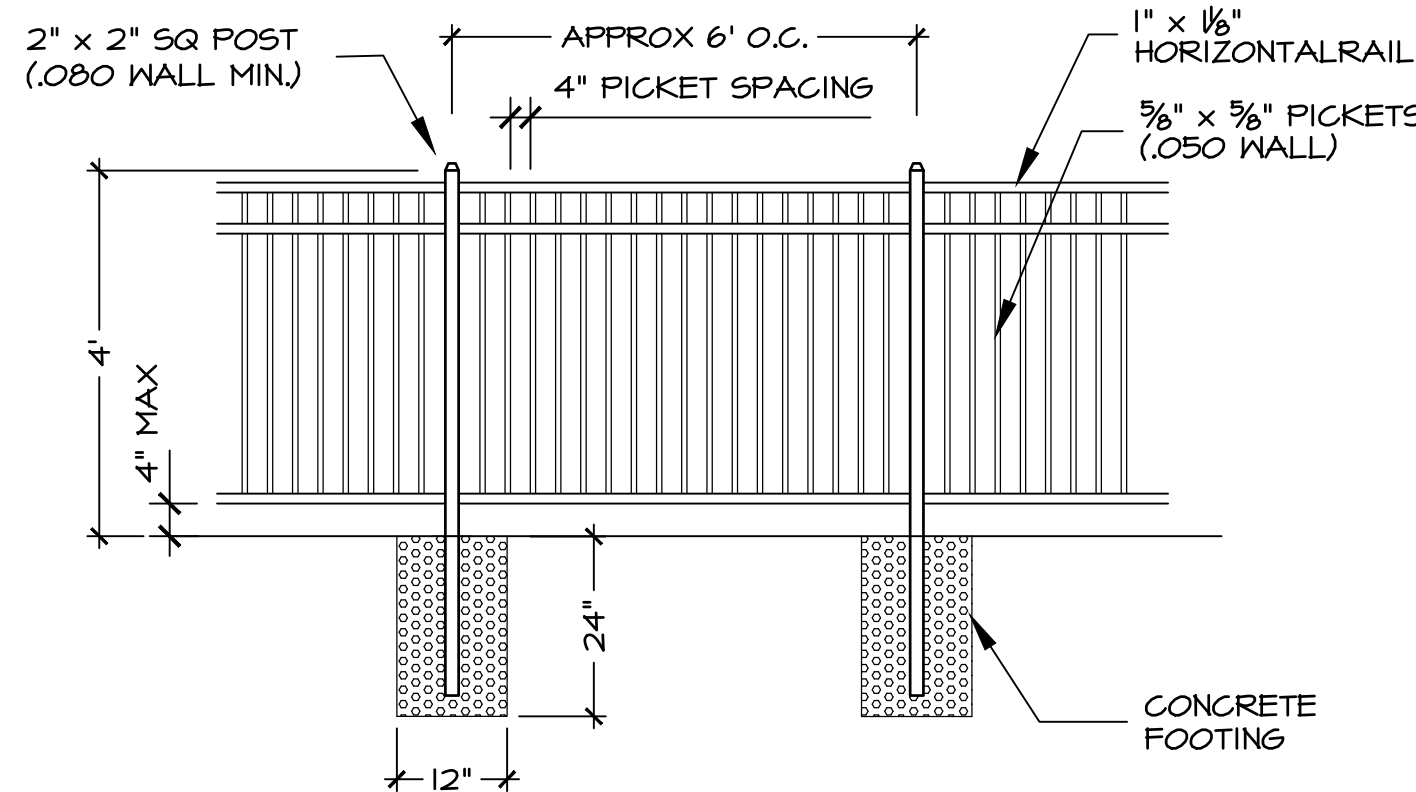
NOT TO SCALE



- NOTES:
1. FENCE TO STEP DOWN TO 3' HEIGHT 20' FROM ROW.

### 4 OPEN VISION VINYL SLAT TOP FENCE

NOT TO SCALE



- NOTES:
1. WROUGHT IRON FENCE STYLE MAY VARY SLIGHTLY.
  2. FENCE TO STEP DOWN TO 3' HEIGHT 20' FROM ROW.

### 5 DECORATIVE IRON FENCE

NOT TO SCALE

## LANDSCAPE CALCULATIONS

COLLECTOR: TWO (2) SHADE TREES AND TWO (2) EVERGREEN TREES PER 100' LINEAR FEET.

| LOCATION                            | BUFFER WIDTH | LENGTH                                   | REQUIRED      | PROVIDED  |
|-------------------------------------|--------------|--|---------------|---|
| N. PALMER LANE                      | 50'          | 932' / 100' =                            | 19 TREES      | 20 TREES<br>(14 SHADE TREES +<br>12 ORNAMENTAL TREES)   |
|                                     |              |  | 19 EVERGREENS | 20 EVERGREENS   |
| W. WILDER FARMS DR.<br>(NORTH SIDE) | 25'          | 1350' / 100' =                           | 27 TREES      | 32.5 TREES<br>(27 SHADE TREES +<br>11 ORNAMENTAL TREES) |
|                                     |              |  | 27 EVERGREENS | 27 EVERGREENS   |
| W. WILDER FARMS DR.<br>(SOUTH SIDE) | 25'          | IRRIGATION EASEMENT<br>NO TREE PERMITTED |               |   |
| COMMON OPEN SPACE                   |              |  |               | 275 TREES   |
| TOTAL NUMBER OF TREES:              |              |  |               | 386 TREES   |

## PLANT PALETTE

| SYM                                  | COMMON NAME                | BOTANICAL NAME                          | SIZE                |
|--------------------------------------|----------------------------|---|---------------------|
| EVERGREEN TREES                      |                            |   |                     |
|                                      | BLACK HILLS SPRUCE         | PICEA GLAUCA 'DENSATA'                  | 6-8' HT B4B         |
|                                      | FAT ALBERT BLUE SPRUCE     | PICEA FUNGENS 'FAT ALBERT'              | 6-8' HT B4B         |
|                                      | MOONGLOW JUNIPER           | JUNIPERUS SCOPULORUM 'MOOGLOW'          | 6-8' HT B4B         |
|                                      | NORWAY SPRUCE              | PICEA ABIES                             | 6-8' HT B4B         |
|                                      | VANDERWOLFS PINE           | PINUS FLEXILIS 'VANDERWOLFS'            | 6-8' HT B4B         |
| SHADE TREES (CLASS III)              |                            |   |                     |
|                                      | BLOODGOOD LONDON PLANETREE | PLATANUS x ACERIFOLIA 'BLOODGOOD'       | 2" CAL B4B          |
|                                      | SWAMP OAK                  | QUERCUS BICOLOR                         | 2" CAL B4B          |
| SHADE/STREET TREES (CLASS II)        |                            |   |                     |
|                                      | CRIMSON SPIRE OAK          | QUERCUS ROBUR x Q. ALBA 'CRIMSCHMIDT'   | 2" CAL B4B          |
|                                      | CHANTICLEER PEAR           | PYRUS CALLERYANA 'GLEN'S FORM'          | 2" CAL B4B          |
|                                      | SKYLINE HONEYLOCUST        | GLEDITSIA TRIACANTHOS INERMIS 'SKYCOLE' | 2" CAL B4B          |
|                                      | LITTLELEAF LINDEN          | TILIA CORDATA                           | 2" CAL B4B          |
|                                      | TULIP TREE                 | LIRODENDRON TULIPIFERA                  | 2" CAL B4B          |
| ORNAMENTAL TREES (CLASS I)           |                            |   |                     |
|                                      | CANADA RED CHOKECHERRY     | PRUNUS VIRGINIANA 'CANADA RED'          | 6-8' HT. MULTI-STEM |
|                                      | CRUZAN CRUSADER HAWTHORN   | CRATAEGUS CRUS-GALLI 'CRUZAM'           | 2" CAL B4B          |
|                                      | ACER TATARICUM 'GARANN'    |   | 6-8' HT. MULTI-STEM |
|                                      | ROYAL RAINDROPS CRABAPPLE  | MALUS x 'JFS-K4B'                       | 2" CAL B4B          |
|                                      | SPRING SNOW CRABAPPLE      | MALUS 'SPRINGSNOW'                      | 2" CAL B4B          |
| SHRUBS/ORNAMENTAL GRASSES/PERENNIALS |                            |   |                     |
|                                      | BLACK EYED SUSAN           | RUDBECKIA FULGIDA 'GOLDSTRUM'           | 1 GAL               |
|                                      | BLUE OAT GRASS             | HELIOTRICHON SEMPERVIRENS               | 1 GAL               |
|                                      | BLUE RUG JUNIPER           | JUNIPERUS HORIZONTALIS 'WILTON'         | 3 GAL               |
|                                      | PURPLE CONEFLOWER          | ECHINACEA PURPUREA                      | 1 GAL               |
|                                      | RED FLOWER CARPET ROSE     | ROSA 'FLOWER CARPET' NOARE              | 2 GAL               |
|                                      | DARTS GOLD NINEBARK        | PHYSOCARPUS OPULIFOLIUS 'DART'S GOLD'   | 5 GAL               |
|                                      | STELLA DE ORO DAYLILY      | HEMEROCALLIS 'STELLA D'ORO'             | 1 GAL               |
|                                      | FINE LINE BUCKTHORN        | RHAMNUS FRAGULA 'RON WILLIAMS'          | 5 GAL               |
|                                      | GRO-LOW SUMAC              | RHUS AROMATICA 'GRO-LOW'                | 5 GAL               |
|                                      | HUSKER RED PENSTEMON       | PENSTEMON DIGITALIS 'HUSKER RED'        | 1 GAL               |
|                                      | IVORY HALO DOGWOOD         | CORNUS ALBA 'BAILHALO'                  | 5 GAL               |
|                                      | KARL FOERSTER REED GRASS   | CALAMAGROSTIS ARUNDINACEA 'K.F.'        | 1 GAL               |
|                                      | MAIDEN GRASS               | MISCANTHUS SINENSIS 'GRACILLIMUS'       | 1 GAL               |
|                                      | BRAKELIGHTS RED YUCCA      | HESPERALOE PARVIFLORA 'PERPA'           | 1 GAL               |
|                                      | SUMMERWINE NINEBARK        | PHYSOCARPUS OPULIFOLIA 'SEWARD'         | 5 GAL               |

- LAWN SOD
- 6' VINYL SLAT TOP FENCE. SEE DTL 4, THIS SHT. (TYP)
- 5' OPEN VISION IRON FENCE ADJACENT TO COMMON SPACES. SEE DTL 5, THIS SHT. (TYP)

## NOTES

1. ALL LANDSCAPE SHALL BE INSTALLED IN ACCORDANCE WITH THE CITY OF STAR ORDINANCE REQUIREMENTS.
2. ALL PLANTING AREAS TO BE WATERED WITH AN AUTOMATIC UNDERGROUND IRRIGATION SYSTEM.
3. TREES SHALL NOT BE PLANTED WITHIN THE 10-FOOT CLEAR ZONE OF ALL STORM DRAIN PIPE, STRUCTURES, OR FACILITIES. SEEPAGE BEDS MUST BE PROTECTED FROM ANY AND ALL CONTAMINATION DURING THE CONSTRUCTION AND INSTALLATION OF THE LANDSCAPE IRRIGATION SYSTEM. ALL SHRUBS PLANTED OVER OR ADJACENT TO SEEPAGE BEDS TO HAVE A ROOT BALL THAT DOES NOT EXCEED 18" IN DIAMETER. NO LAWN SOD TO BE PLACED OVER DRAINAGE SWALE SAND WINDOWS.
4. NO TREES SHALL IMPEDE THE 40' VISION TRIANGLES AT ALL INTERSECTIONS. NO CONIFEROUS TREES OR SHRUBS OVER 3' HIGH AT MATURITY WILL BE LOCATED WITHIN TRIANGLE OR ROW. AS TREES MATURE, THE OWNER SHALL BE RESPONSIBLE FOR PRUNING TREE CANOPIES TO MEET REQUIREMENTS FOR MAINTAINING CLEAR VISIBILITY WITHIN 40' STREET AND DEPARTURE VISION TRIANGLE. TREES SHALL BE PLANTED NO CLOSER THAN 50' FROM INTERSECTION STOP SIGNS.
5. CLASS II TREES AND LANDSCAPE IN FRONT OF BUILDING LOTS ON INTERIOR STREETS TO BE COMPLETED DURING CONSTRUCTION ON THESE LOTS. TREE LOCATIONS MAY BE ALTERED TO ACCOMMODATE DRIVEWAYS AND UTILITIES. TREES MUST BE CLASS II AND SHALL NOT BE PLANTED WITHIN 5' OF WATER METERS OR UNDERGROUND UTILITY LINES. BUILDER SHALL BE REQUIRED TO INSTALL STREET TREES 5' FROM BACK OF SIDEWALK EVERY 35' ADJACENT TO ALL BUILDABLE HOME LOTS PRIOR TO OCCUPANCY. TREE SPACING, QUANTITIES, AND PLACEMENT TO BE ADJUSTED FOR DRIVEWAYS AND UTILITY CONFLICTS. TREES TO BE SELECTED FROM SHADE/STREET TREES (CLASS II) LISTED IN PLANT SCHEDULE AS SHOWN ON THIS SHEET.
6. PLANT LIST IS REPRESENTATIVE AND SUBJECT TO ADDITIONS AND/OR SUBSTITUTIONS OF SIMILAR SPECIES THAT ARE SUBJECT TO CITY FORESTER'S PRE-APPROVAL. PLANTING BED DESIGN AND QUANTITIES MAY BE ALTERED DURING FINAL PLAT LANDSCAPE PLAN DESIGN. BURLAP AND WIRE BASKETS TO BE REMOVED FROM ROOT BALL AS MUCH AS POSSIBLE, AT LEAST HALFWAY DOWN THE BALL OF THE TREE. ALL NYLON ROPES TO BE COMPLETELY REMOVED FROM TREES.
7. EXISTING TREES ADJACENT TO DRAINS SHALL BE RETAINED.

## DEVELOPMENT DATA

|                       |                      |
|-----------------------|----------------------|
| TOTAL AREA            | 60.52 ACRES          |
| RESIDENTIAL LOTS      | 173                  |
| TOWNHOUSE RESIDENTIAL | 78                   |
| OPEN SPACE LOTS       | 27                   |
| TOTAL LOTS            | 278                  |
| USEABLE OPEN SPACE    | 10.96 ACRES (18.11%) |
| EXISTING ZONING       | RUT                  |
| PROPOSED ZONING       | R-5                  |

| DEVELOPER   | PLANNER/CONTACT  |
|---|--|
| CHALLENGER DEVELOPMENT, INC.<br>1977 E. OVERLAND ROAD<br>MERIDIAN, ID 83642<br>Phone (208) 288-5560 | ENGINEERING SOLUTIONS<br>BECKY MCKAY<br>1029 N. ROSARIO ST., STE 100<br>MERIDIAN, ID 83642<br>Phone (208) 938-0980 |

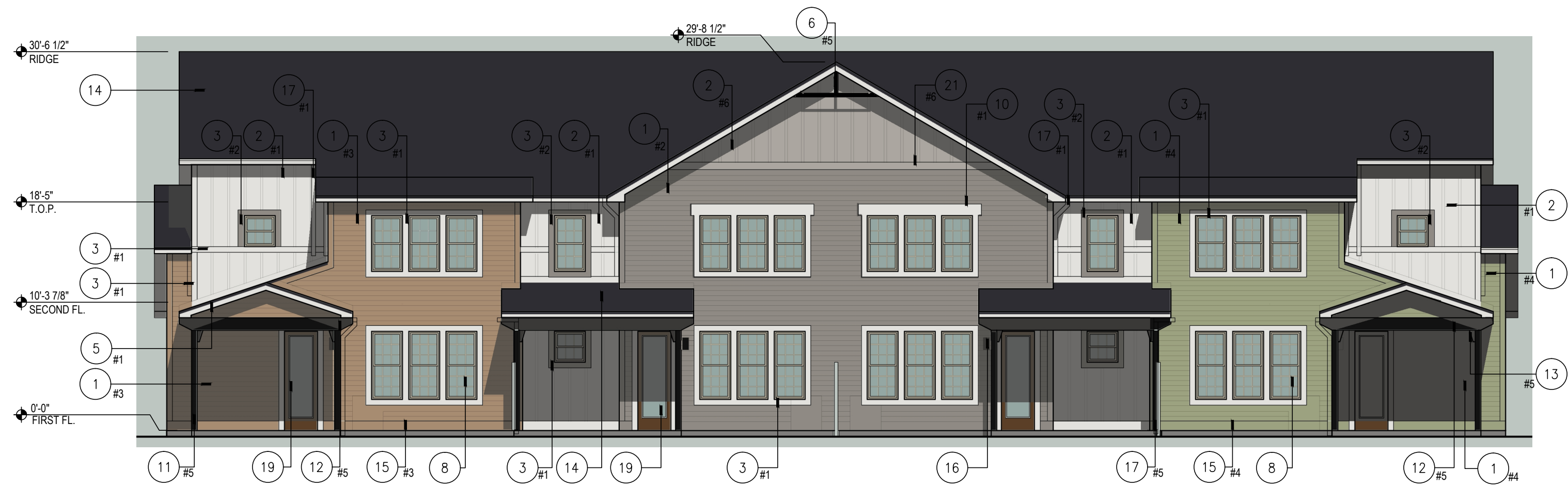
**JENSEN BELTS ASSOCIATES**

Site Planning / Landscape Architecture

1509 Tyrrell Lane, Ste 130 Boise, ID 83708  
PH. (208) 345-7175 www.jensenbelts.com

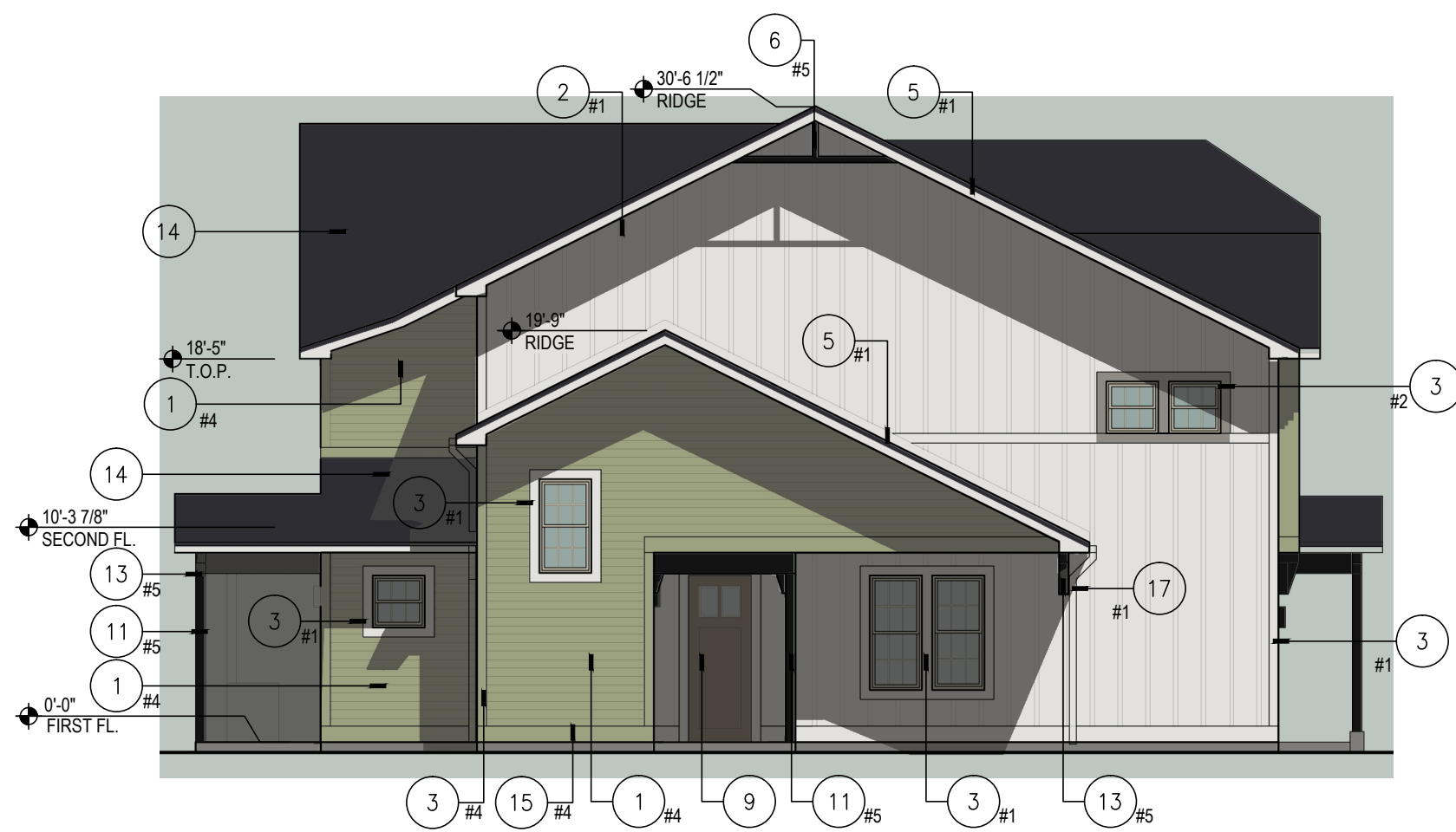
| <b>OWNERS OF RECORD</b>  | OPEN DOOR RENTALS, LLC<br>1977 E. OVERLAND ROAD<br>MERIDIAN, ID 83642<br>PHONE: (208) 288-5560   |          |             |      |    |   |                |          |     |
|--|--|----------|-------------|------|----|---|----------------|----------|-----|
| <b>DEVELOPER</b>   | CHALLENGER DEVELOPMENT, INC.<br>1977 E. OVERLAND ROAD<br>MERIDIAN, ID 83642<br>PHONE: (208) 288-5560   |          |             |      |    |   |                |          |     |
| <b>PLANNER</b>   | BECKY MCKAY<br>ENGINEERING SOLUTIONS<br>1029 N. ROSARIO STREET, STE. 100<br>MERIDIAN, ID 83642<br>PHONE: (208) 938-0980<br>FAX: (208) 938-0941<br>EMAIL: beckym@engsol.org |          |             |      |    |   |                |          |     |
|  |  |          |             |      |    |   |                |          |     |
| <b>REVISIONS</b>   | <table><tr><th>NO.</th><th>DESCRIPTION</th><th>DATE</th><th>BY</th></tr><tr><td>1</td><td>BASE REVISIONS</td><td>08-09-21</td><td>KCS</td></tr></table>                    | NO.      | DESCRIPTION | DATE | BY | 1 | BASE REVISIONS | 08-09-21 | KCS |
| NO.  | DESCRIPTION  | DATE     | BY          |      |    |   |                |          |     |
| 1  | BASE REVISIONS   | 08-09-21 | KCS         |      |    |   |                |          |     |
| <b>ENGINEERING SOLUTIONS LP</b>  | 1029 N. ROSARIO STREET, SUITE 100<br>MERIDIAN, IDAHO 83642<br>PHONE: (208) 938-0980 FAX: (208) 938-0941  |          |             |      |    |   |                |          |     |
| <b>FOUNTAIN PARK SUBDIVISION</b><br>LOCATED IN THE N 1/2 OF THE SW 1/4 OF SECTION 10, T.4N., R.1W., B.M. | <b>PRELIMINARY PLAT LANDSCAPE PLAN</b>   |          |             |      |    |   |                |          |     |
| DATE ISSUED  | 05-12-21   |          |             |      |    |   |                |          |     |
| PROJECT NO.  | JBA-2090   |          |             |      |    |   |                |          |     |
| DWG. FILE  | Fountain Park.dwg  |          |             |      |    |   |                |          |     |
| SCALE  | AS SHOWN   |          |             |      |    |   |                |          |     |
| DRAWN BY   | JUN  |          |             |      |    |   |                |          |     |
| CHECKED BY   | KCS  |          |             |      |    |   |                |          |     |
| <b>L4</b>  | SHEET 05 OF 05   |          |             |      |    |   |                |          |     |





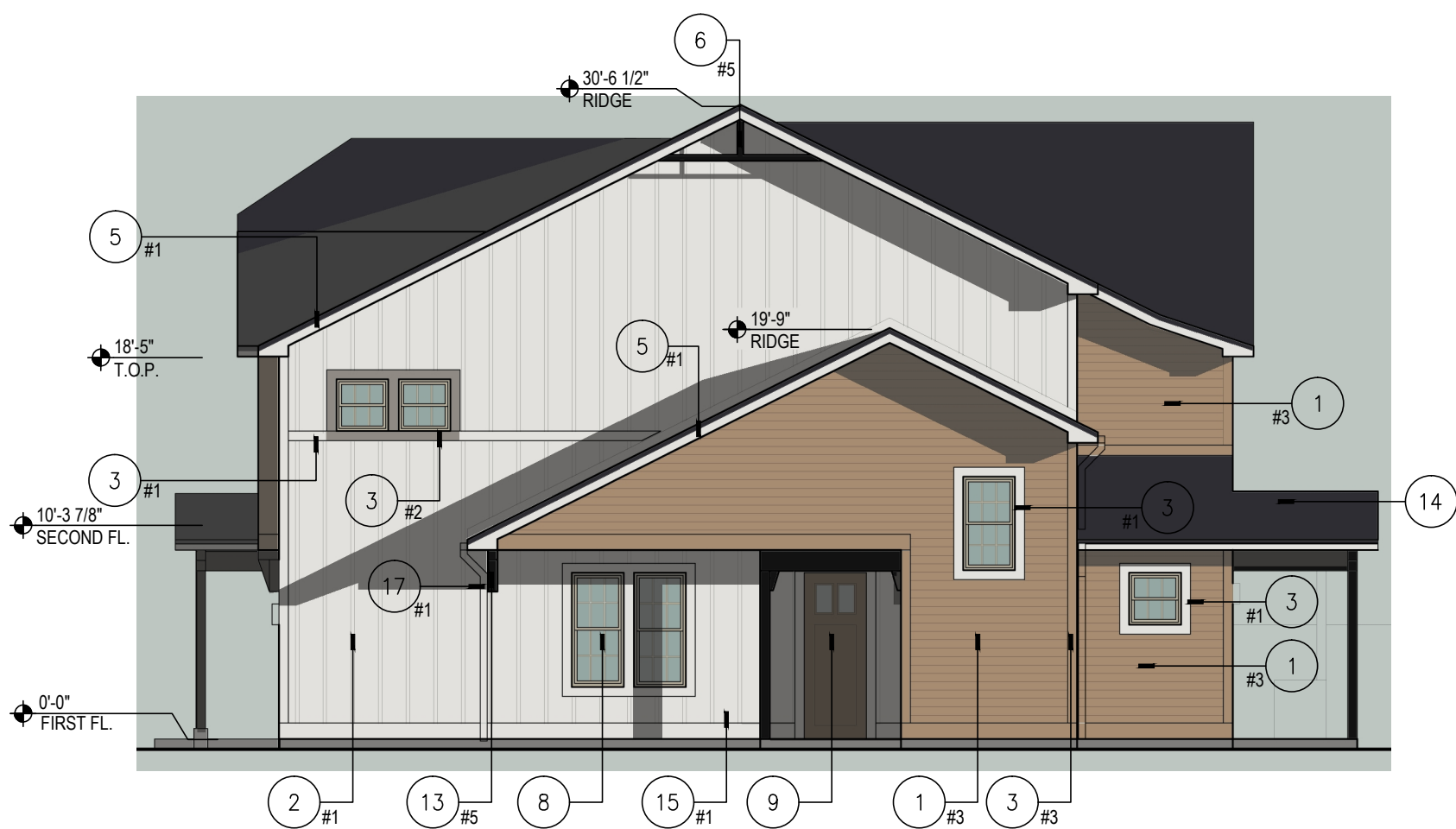
### Rear Elevation

scale: 1/8" = 1'-0"



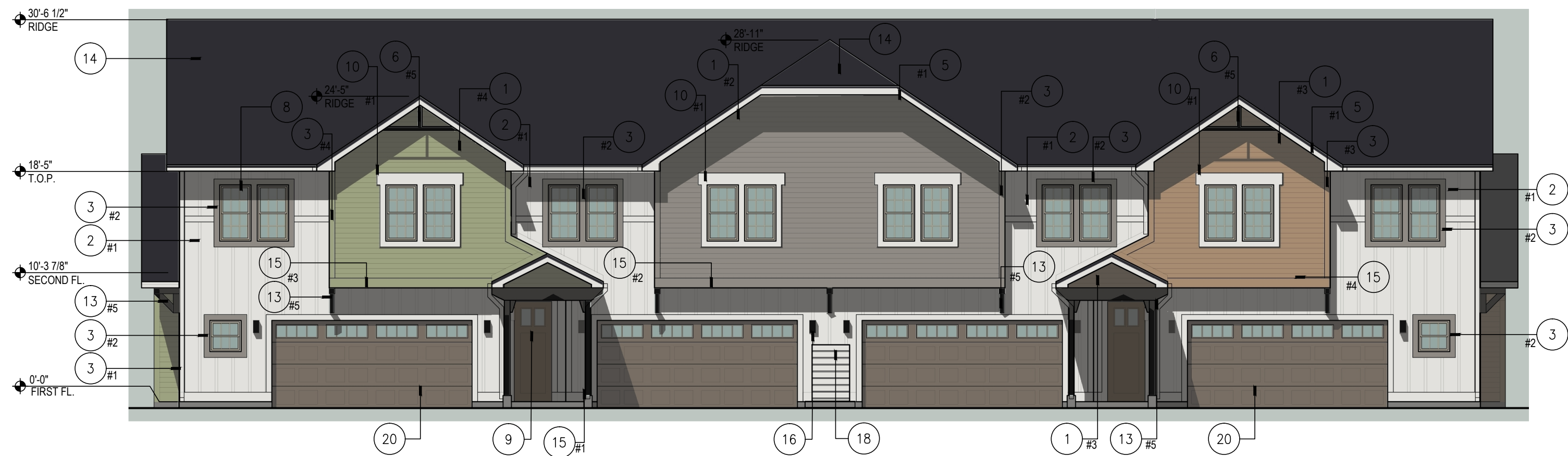
Left Elevation

scale: 1/8" = 1'-0"



### Right Elevation

scale: 1/8" = 1'-0"



### Front Elevation

scale: 1/8" = 1'-0"

## SHEET NOTES (#)

1. LAP SIDING LP SmartSide: SMOOTH FINISH, 5.84" (4.84" EXPOSED) X .375", COLOR AS INDICATED.
2. VERTICAL SIDING LP SmartSide: CEDAR FINISH, 15.94" X .315", NO GROOVE, WITH 2.7" .625" REVERSIBLE TRIM AT 16" O.C. COLOR AS INDICATED.
3. TRIM LP SmartSide: 5.5" X .625", SMOOTH FINISH, COLOR AS INDICATED.
4. FASCIAS LP SmartSide: 7.21" X .625" CUT TO 6" VERTICAL, SMOOTH FINISH, COLOR AS INDICATED.
5. RAKES LP SmartSide: 7.21" X .625", SMOOTH FINISH, COLOR AS INDICATED.
6. GABLE TRIM LP SmartSide: 3.5" X .625", SMOOTH FINISH, COLOR AS INDICATED.
7. SOFFIT LP SmartSide: SMOOTH FINISH, CUT TO WIDTH, VENTILATED
8. WINDOWS: CASCADE VINYL, SINGLE HUNG AND CASEMENT, "CLAY" EXTERIOR FINISH, "FLAT" GRID PROFILE
9. ENTRY DOORS: THERMA-TRU PAINTED FIBERGLASS, "CLASSIC CRAFT AMERICAN STYLE" CCA820-SPL, COLOR: DRIFTWOOD
10. HEAD TRIM LP SmartSide: 11.21" X .625", SMOOTH FINISH, COLOR AS INDICATED.
11. COLUMNS: PAINTED WOOD, COLOR AS INDICATED.
12. BEAM-PAINTED WOOD, COLOR AS INDICATED.
13. BRACKET: PAINTED WOOD, COLOR AS INDICATED.
14. ASPHALT SHINGLE ROOFING, PASCO 30-YEAR OR SIMILAR, COLOR: ANTIQUE BLACK
15. BASE TRIM LP SmartSide: 9.21" X .910", SMOOTH FINISH, COLOR AS INDICATED.
16. LANTERN STYLE LIGHT FIXTURE.
17. GUTTERS AND DOWNSPOUTS, COLOR AS INDICATED.
18. GAS METER SCREEN/OPENABLE: 3" WIDE X 4'-6" TALL, LP SmartSide: 5.5" X .625" SMOOTH FINISH TRIM RAN HORIZONTAL WITH 1" SPACE BETWEEN. PAINT TO MATCH WALL SIDING BEHIND.
19. PATIO DOOR.
20. GARAGE DOORS: AMAROK OAK SUMMIT, S2000 SERIES INSULATED STEEL, "BB30 BEAD BOARD LONG PANEL THAMES" COLOR: TERRATONE.
21. TRIM LP SmartSide: 7.21" X .625", SMOOTH FINISH, COLOR AS INDICATED.

MATERIAL/COLOR SCHEDULE:

|   |   |
|---|---|
|    | MATERIAL: AS INDICATED<br>COLOR #1: SW6070/HERON PLUME            |
|    | MATERIAL: AS INDICATED<br>COLOR #2: SW7018/DOVETAIL               |
|   | MATERIAL: AS INDICATED<br>COLOR #3: SW2823/ROOKWOOD CLAY          |
|  | MATERIAL: AS INDICATED<br>COLOR #4: SW2826/COLONIAL REVIVAL GREEN |
|  | MATERIAL: AS INDICATED<br>COLOR #5: SW6988/BOHEMIAN BLACK         |
|  | MATERIAL: AS INDICATED<br>COLOR #6: SW7017/DORIAN GRAY            |
|  | MATERIAL: GARAGE DOORS<br>COLOR: TERRATONE                        |
|  | MATERIAL: ENTRY DOOR<br>COLOR: DRIFTWOOD                          |
|  | MATERIAL: VINYL WINDOWS<br>COLOR: CLAY                            |

NOT FOR  
CONSTRUCTION

|                |              |
|----------------|--------------|
| ISSUE/REVISION |              |
|                |              |
|                |              |
|                |              |
|                |              |
|                |              |
|                |              |
|                |              |
| MCKC JOB NO.   | 1979a400     |
|                |              |
|                |              |
| DATE           | 24 FEB 2020  |
| SCALE          | AS INDICATED |
| DRAWN          | CMS          |
| CHECKED        | DGS          |

## DESIGN REVIEW



Front Perspective - Right Side Corner



Front Perspective - Left Side Corner



## COREY BARTON HOMES

Union Square Townhomes  
Union Square Subdivision No. 4  
Lots 34-37 and 38-41





Rear Perspective - Right Side Corner



Rear Perspective - Left Side Corner



## COREY BARTON HOMES

Union Square Townhomes  
Union Square Subdivision No. 4  
Lots 34-37 and 38-41







August 19, 2021

Mayor Trevor Chadwick  
City of Star  
P.O. Box 130  
Star, ID 83669

**Re: Fountain Park Subdivision Preliminary Plat Application**

Dear Mayor:

Keller Associates, Inc. has reviewed the Preliminary Plat for the Fountain Park Subdivision dated April 28, 2021. We reviewed the applicant's package to check conformance with the City's Subdivision Ordinance and coordinated our review with Shawn L. Nickel. We have the following comments and questions based on our review.

1. The legal description we were provided does not match along the northern boundary. It does not include any property north of the long east-west property line.
2. There are no landscaping plans included, and this is required for preliminary plats. Landscape plans including fencing, buffer areas, and street trees will have to conform to the City subdivision ordinance. Landscape plans including fencing, buffer areas, and street trees will have to conform to the City subdivision ordinance.
3. It is unclear the intent for the stormwater improvement for Palmer Lane; please provide additional information.
4. Potable water cannot be used for irrigation purposes. A separate pressure irrigation system will be required. Construction plans for a subdivision-wide pressure irrigation system will be required for each final plat. Plan approvals and license agreements from the affected irrigation and/or canal companies will be required.
5. Historic irrigation lateral, drain, and ditch flow patterns shall be maintained unless approved in writing by the local irrigation district or ditch company.
6. Finish grades at subdivision boundaries shall match existing finish grades. Runoff shall be maintained on subdivision property unless otherwise approved.
7. 10-foot easements for pressure irrigation lines will need to be shown once the applicant determines the alignment location(s) for the facilities. Show all ditch and drainage easements.
8. Easements for sewer/water facilities will be required where placed outside of the public right of way.



We recommend that **conditions 1 – 3 listed above be addressed prior to approval of the Preliminary Plat.** Any variance or waivers to the City of Star standards, ordinances, or policies must be specifically approved in writing by the City. Approval of the above-referenced Preliminary Plat does not relieve the Registered Professional Land Surveyor or the Registered Professional Engineer of those responsibilities.

If you have any questions, please do not hesitate to call Keller Associates at (208) 288-1992.

Sincerely,

**KELLER ASSOCIATES, INC.**



Ryan V. Morgan, P.E.  
City Engineer

cc: File



**Boise Office**  
Golden Eagle Building  
1101 W. River St., Ste. 110  
P.O. Box 7985  
Boise, Idaho 83707  
Tel. (208) 629-7447

**Challis Office**  
1301 E. Main Ave.  
P.O. Box 36  
Challis, Idaho 83226  
Tel. (208) 879-4488

**Twin Falls Office**  
236 River Vista Place  
Suite 301  
Twin Falls, Idaho 83301  
Tel. (208) 969-9585

Fax (208) 629-7559



# SAWTOOTH LAW OFFICES, PLLC

**David P. Claiborne \***

**S. Bryce Farris**

**Patxi Larrocea-Phillips**

**Evan T. Roth**

**Daniel V. Steenson**

**Matthew A. Sturzen**

**Katie L. Vandenberg-Van  
Vliet**

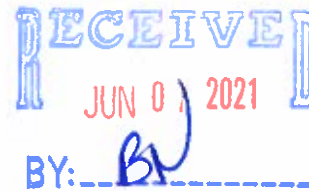
**Andrew J. Waldera \*\***

**James R. Bennetts (retired)**

*Attorneys licensed in Idaho  
\* Also licensed in Washington  
\*\* Also licensed in Oregon*

Wednesday, June 02, 2021

City of Star  
Attn: Shawn L. Nickel  
P.O. Box 130  
Star, Idaho 83669



Re: File Nos # AZ-21-19, DA-21-24 and PP-21-17 - **Fountain Park Subdivision** located on the west side of N. Palmer Lane, Approx. ¼ mile north of W. State Street in Star, Idaho

Dear Mr. Nickel:

The Drainage District No. 2 (referred to as "District") has a ditch or drain and easement that runs through or abuts this property. The District's easement is 100 feet, 50 feet to either side for open drains and 50 feet, 25 feet to either side for piped or closed drains. In addition, the Middleton Mill Ditch Company and Middleton Irrigation Association (collectively referred to as "MM-MI") have a lateral ditch and easement that runs through or abuts this property. The easement is 25 feet each side from the top of bank. The developer must contact the District and MM-MI's attorneys, Sawtooth Law Offices, PLLC, for approval before any encroachment, change of easement, or drainage discharge into District or MM-MI facilities occurs. The District and MM-MI must review drainage plans and construction plans prior to any approval.

The District and MM-MI generally require a License Agreement prior to any approval for the following reasons:

1. Relocation of a facility which would also require a new easement and relinquishment of the old easement once the relocation has been completed.
2. Piping of a facility.
3. Encroachment on a facility with gas, water and sewer lines, utility lines, roadways, bridges or any other structures.
4. Drainage discharges into any facilities.





June 2, 2021  
City of Star Itr.  
Page 2

Also, please be advised that neither the District or MM-MI approves of trees within their easements. Therefore, any existing trees within easement will need to be removed. On occasion, the District and MM-MI may make exceptions on a case-by-case basis, which requires the developers/owners to obtain written permission from the District or MM-MI for existing trees to remain.

Please contact me if you have any questions.

Yours very truly,

A handwritten signature in blue ink, appearing to read "S. Bryce Farris", with a long horizontal flourish extending to the right.

S. Bryce Farris

SBF:krk  
cc: MM-MI/DD2

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MIDDLETON RURAL FIRE DISTRICT

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---



Fountain Park Subdivision

---

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STAR FIRE PROTECTION DISTRICT

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DATE: August 27, 2021

TO: City of Star, Planning & Zoning

FROM: Victor Islas, Deputy Chief

SUBJECT: Fire District Review

PROJECT NAME: Fountain Park Subdivision (AZ-21-19, DA-21-24-24, PP-21-17)

**Fire District Summary Report:**

**Overview:** This development can be serviced by the Star Fire Protection District. This development shall comply with the 2018 International Fire Code (IFC) and any codes set forth by the City of Star, Idaho.

**Fire Response Time:** This development will be served by the Star Fire Protection District Station 51, located at 11655 W. State St., Star, Idaho. Station 51 is 3.1 miles with a travel time of 7 minutes under ideal driving conditions to the proposed entrance of the development N. Palmer Lane.

**Accessibility:** Roadway Access, Traffic, Radio Coverage

Access roads shall be provided and maintained following Appendix D and Section 503 of the IFC. Access shall include adequate roadway widths, signage, turnarounds, and turning radius for fire apparatus.

Access road design shall be designed and constructed to allow for evacuation simultaneously with emergency response operations.

All access roads in this development shall remain clear and unobstructed during construction of the development. Additional parking restrictions may be required as to always maintain access for emergency vehicles. Hydrants shall always remain unobstructed per city code.

One- or two-family dwelling residential developments: Developments of one- or two-family dwellings where the number of dwelling units exceeds 30 shall provide with at least two separate and approved fire apparatus access roads.

**Review note:** *Additional Access will be required off Pollard Lane.*

The fire district requires that Autoturn models be submitted for review. Autoturn models should be reflect the utilization of a 36' long fire engine and a 50' long ladder truck.

Traffic calming devices will require approval by the Fire District.

An unobstructed vertical clearance of no less than 13 feet 6 inches shall be always maintained.



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**MIDDLETON RURAL FIRE DISTRICT**

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Fountain Park Subdivision

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**STAR FIRE PROTECTION DISTRICT**

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The applicant shall work with City of Star, Ada County and Fire District to provide an address identification plan and signage which meets the requirements set forth by each agency. Addressing shall be placed in a position that is plainly legible and visible from the street or road fronting the property, as set forth in International Fire Code Section 505.1

Upon commencement of initial construction of a new structure, a clear visible freestanding sign or post shall be erected and maintained in place until the permanent address numerals are attached or otherwise displaced upon the premises at completion.

Dead-end fire apparatus access road turnarounds will be required for the two purposed shared driveways.

**Water Supply:**

Water supply requirements will be followed as described in Appendix B of the 2015 International Fire Code unless agreed upon by the Fire District.

1. Fire Flow: One- and two-family dwellings not exceeding 3,600 square feet require a fire-flow of 1,000 gallons per minute for a duration of 1 hours to service the entire project. One- and two-family dwellings in excess of 3,600 square feet require a minimum fire flow as specified in Appendix B of the International Fire Code.
2. Water Supply: Acceptance of the water supply for fire protection will be by the Fire District and water quality by the Star Sewer & Water District for bacteria testing.
3. Water Supply: Final Approval of the fire hydrant locations shall be by the Star Fire Protection District or their designee in accordance with International Fire Code Section (IFC) 508.5.4 as follows:
  - a. Fire hydrants shall have a Storz LDH connection in place of the 4 ½" outlet. The Storz connection may be integrated into the hydrant, or an approved adapter may be used on the 4 1/2" outlet.
  - b. Fire hydrants shall have the Storz outlet face the main street or parking lot drive aisle.
  - c. Fire hydrants shall be placed on corners when spacing permits.
  - d. Fire hydrants shall not have any vertical obstructions to outlets within 10'.
  - e. Fire hydrants shall be placed 18" above finished grade to the center of the Storz outlet.
  - f. Fire hydrants shall be provided to meet the requirements of the City of Star and Star Sewer and Water District Standards.
  - g. Show all proposed or existing hydrants for all new construction or additions to existing buildings within 1,000 feet of the project.

**Inspections:**

Final inspection by the Fire District of the above listed including hydrant flow must be completed before building permits are issued

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MIDDLETON RURAL FIRE DISTRICT

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Fountain Park Subdivision

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STAR FIRE PROTECTION DISTRICT

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**Additional Comments:**

Side Setback as per City Code. Any modification to setback will require review and approval by the Fire District.

Streetlights shall be turned on once residential building begins, Lighting is essential in assisting first responders with identifying entrances safely while responding to calls for service.

Additional fire and life safety review and permitting will be required for commercial buildings, multi-family and townhomes.





**STATE OF IDAHO**  
DEPARTMENT OF  
ENVIRONMENTAL QUALITY

1445 N Orchard Street, Boise, ID 83706  
(208) 373-0550

Brad Little, Governor  
Jess Byrne, Director

June 11, 2021

By e-mail: [Snickel@staridaho.org](mailto:Snickel@staridaho.org)

City of Star  
P.O. Box 130  
Star, Idaho 83669

Subject: Fountain Park Subdivision, AZ-21-19/DA-21-24/PP-21-17

Dear Mr. Nickel:

Thank you for the opportunity to respond to your request for comment. While DEQ does not review projects on a project-specific basis, we attempt to provide the best review of the information provided. DEQ encourages agencies to review and utilize the Idaho Environmental Guide to assist in addressing project-specific conditions that may apply. This guide can be found at:

<https://www.deq.idaho.gov/public-information/assistance-and-resources/outreach-and-education/>

The following information does not cover every aspect of this project; however, we have the following general comments to use as appropriate:

**1. AIR QUALITY**

- Please review IDAPA 58.01.01 for all rules on Air Quality, especially those regarding fugitive dust (58.01.01.651), trade waste burning (58.01.01.600-617), and odor control plans (58.01.01.776).
- All property owners, developers, and their contractor(s) must ensure that reasonable controls to prevent fugitive dust from becoming airborne are utilized during all phases of construction activities per IDAPA 58.01.01.651.
- DEQ recommends the city/county require the development and submittal of a dust prevention and control plan for all construction projects prior to final plat approval. Dust prevention and control plans incorporate appropriate best management practices to control fugitive dust that may be generated at sites.
- Citizen complaints received by DEQ regarding fugitive dust from development and construction activities approved by cities or counties will be referred to the city/county to address under their ordinances.

- Per IDAPA 58.01.01.600-617, the open burning of any construction waste is prohibited. The property owner, developer, and their contractor(s) are responsible for ensuring no prohibited open burning occurs during construction.
- For questions, contact David Luft, Air Quality Manager, at (208) 373-0550.

## **2. WASTEWATER AND RECYCLED WATER**

- DEQ recommends verifying that there is adequate sewer to serve this project prior to approval. Please contact the sewer provider for a capacity statement, declining balance report, and willingness to serve this project.
- IDAPA 58.01.16 and IDAPA 58.01.17 are the sections of Idaho rules regarding wastewater and recycled water. Please review these rules to determine whether this or future projects will require DEQ approval. IDAPA 58.01.03 is the section of Idaho rules regarding subsurface disposal of wastewater. Please review this rule to determine whether this or future projects will require permitting by the district health department.
- All projects for construction or modification of wastewater systems require preconstruction approval. Recycled water projects and subsurface disposal projects require separate permits as well.
- DEQ recommends that projects be served by existing approved wastewater collection systems or a centralized community wastewater system whenever possible. Please contact DEQ to discuss potential for development of a community treatment system along with best management practices for communities to protect ground water.
- DEQ recommends that cities and counties develop and use a comprehensive land use management plan, which includes the impacts of present and future wastewater management in this area. Please schedule a meeting with DEQ for further discussion and recommendations for plan development and implementation.

For questions, contact Valerie Greear, Water Quality Engineering Manager at (208) 373-0550.

## **3. DRINKING WATER**

- DEQ recommends verifying that there is adequate water to serve this project prior to approval. Please contact the water provider for a capacity statement, declining balance report, and willingness to serve this project.
- IDAPA 58.01.08 is the section of Idaho rules regarding public drinking water systems. Please review these rules to determine whether this or future projects will require DEQ approval.
- All projects for construction or modification of public drinking water systems require preconstruction approval.
- DEQ recommends verifying if the current and/or proposed drinking water system is a regulated public drinking water system (refer to the DEQ website at: [deg.idaho.gov/water-quality/drinking-water.aspx](http://deg.idaho.gov/water-quality/drinking-water.aspx)). For non-regulated systems, DEQ recommends annual testing for total coliform bacteria, nitrate, and nitrite.



- If any private wells will be included in this project, we recommend that they be tested for total coliform bacteria, nitrate, and nitrite prior to use and retested annually thereafter.
- DEQ recommends using an existing drinking water system whenever possible or construction of a new community drinking water system. Please contact DEQ to discuss this project and to explore options to both best serve the future residents of this development and provide for protection of ground water resources.
- DEQ recommends cities and counties develop and use a comprehensive land use management plan which addresses the present and future needs of this area for adequate, safe, and sustainable drinking water. Please schedule a meeting with DEQ for further discussion and recommendations for plan development and implementation.

For questions, contact Valerie Greear, Water Quality Engineering Manager at (208) 373-0550.

#### 4. SURFACE WATER

- Please contact DEQ to determine whether this project will require a National Pollution Discharge Elimination System (NPDES) Permit. A Construction General Permit from EPA may be required if this project will disturb one or more acres of land, or will disturb less than one acre of land but are part of a common plan of development or sale that will ultimately disturb one or more acres of land.
- If this project is near a source of surface water, DEQ requests that projects incorporate construction best management practices (BMPs) to assist in the protection of Idaho's water resources. Additionally, please contact DEQ to identify BMP alternatives and to determine whether this project is in an area with Total Maximum Daily Load stormwater permit conditions.
- The Idaho Stream Channel Protection Act requires a permit for most stream channel alterations. Please contact the Idaho Department of Water Resources (IDWR), Western Regional Office, at 2735 Airport Way, Boise, or call (208) 334-2190 for more information. Information is also available on the IDWR website at: <https://idwr.idaho.gov/streams/stream-channel-alteration-permits.html>
- The Federal Clean Water Act requires a permit for filling or dredging in waters of the United States. Please contact the US Army Corps of Engineers, Boise Field Office, at 10095 Emerald Street, Boise, or call 208-345-2155 for more information regarding permits.

For questions, contact Lance Holloway, Surface Water Manager, at (208) 373-0550.

#### 5. SOLID WASTE, HAZARDOUS WASTE AND GROUND WATER CONTAMINATION

- **Solid Waste.** No trash or other solid waste shall be buried, burned, or otherwise disposed of at the project site. These disposal methods are regulated by various state regulations including Idaho's Solid Waste Management Regulations and Standards (IDAPA 58.01.06), Rules and Regulations for Hazardous Waste (IDAPA 58.01.05), and Rules and Regulations for the Prevention of Air Pollution (IDAPA 58.01.01). Inert and other approved materials are also defined in the Solid Waste Management Regulations and Standards

- **Hazardous Waste.** The types and number of requirements that must be complied with under the federal Resource Conservation and Recovery Act (RCRA) and the Idaho Rules and Standards for Hazardous Waste (IDAPA 58.01.05) are based on the quantity and type of waste generated. Every business in Idaho is required to track the volume of waste generated, determine whether each type of waste is hazardous, and ensure that all wastes are properly disposed of according to federal, state, and local requirements.
- **Water Quality Standards.** Site activities must comply with the Idaho Water Quality Standards (IDAPA 58.01.02) regarding hazardous and deleterious-materials storage, disposal, or accumulation adjacent to or in the immediate vicinity of state waters (IDAPA 58.01.02.800); and the cleanup and reporting of oil-filled electrical equipment (IDAPA 58.01.02.849); hazardous materials (IDAPA 58.01.02.850); and used-oil and petroleum releases (IDAPA 58.01.02.851 and 852). Petroleum releases must be reported to DEQ in accordance with IDAPA 58.01.02.851.01 and 04. Hazardous material releases to state waters, or to land such that there is likelihood that it will enter state waters, must be reported to DEQ in accordance with IDAPA 58.01.02.850.
- **Ground Water Contamination.** DEQ requests that this project comply with Idaho's Ground Water Quality Rules (IDAPA 58.01.11), which states that "No person shall cause or allow the release, spilling, leaking, emission, discharge, escape, leaching, or disposal of a contaminant into the environment in a manner that causes a ground water quality standard to be exceeded, injures a beneficial use of ground water, or is not in accordance with a permit, consent order or applicable best management practice, best available method or best practical method."

For questions, contact Albert Crawshaw, Waste & Remediation Manager, at (208) 373-0550.

## 6. ADDITIONAL NOTES

- If an underground storage tank (UST) or an aboveground storage tank (AST) is identified at the site, the site should be evaluated to determine whether the UST is regulated by DEQ. EPA regulates ASTs. UST and AST sites should be assessed to determine whether there is potential soil and ground water contamination. Please call DEQ at (208) 373-0550, or visit the DEQ website <https://www.deq.idaho.gov/waste-management-and-remediation/storage-tanks/leaking-underground-storage-tanks-in-idaho/> for assistance.
- If applicable to this project, DEQ recommends that BMPs be implemented for any of the following conditions: wash water from cleaning vehicles, fertilizers and pesticides, animal facilities, composted waste, and ponds. Please contact DEQ for more information on any of these conditions.



Response to Request for Comment

June 11, 2021

Page 5

We look forward to working with you in a proactive manner to address potential environmental impacts that may be within our regulatory authority. If you have any questions, please contact me, or any of our technical staff at (208) 373-0550.

Sincerely,

A handwritten signature in black ink that reads "Aaron Scheff". The signature is written in a cursive, flowing style.

Aaron Scheff  
Regional Administrator  
DEQ-Boise Regional Office

EDMS#: 2021AEK105



Ada County Transmittal  
Division of Community and Environmental Health

Return to:

- ☐ ACZ  
☐ Boise  
☐ Eagle  
☐ Garden City  
☐ Meridian  
☐ Kuna  
☒ Star

Rezone # \_\_\_\_\_

Conditional Use # \_\_\_\_\_

Preliminary / Final / Short Plat PP-21-17

Fountain Park

- ☐ 1. We have No Objections to this Proposal.
- ☐ 2. We recommend Denial of this Proposal.
- ☐ 3. Specific knowledge as to the exact type of use must be provided before we can comment on this Proposal.
- ☐ 4. We will require more data concerning soil conditions on this Proposal before we can comment.
- ☐ 5. Before we can comment concerning individual sewage disposal, we will require more data concerning the depth of:  
☐ high seasonal ground water ☐ waste flow characteristics  
☐ bedrock from original grade ☐ other \_\_\_\_\_
- ☐ 6. This office may require a study to assess the impact of nutrients and pathogens to receiving ground waters and surface waters.
- ☐ 7. This project shall be reviewed by the Idaho Department of Water Resources concerning well construction and water availability.
- ☒ 8. After written approvals from appropriate entities are submitted, we can approve this proposal for:  
☒ central sewage ☐ community sewage system ☐ community water well  
☐ interim sewage ☒ central water  
☐ individual sewage ☐ individual water
- ☒ 9. The following plan(s) must be submitted to and approved by the Idaho Department of Environmental Quality:  
☒ central sewage ☐ community sewage system ☐ community water  
☐ sewage dry lines ☒ central water
- ☐ 10. This Department would recommend deferral until high seasonal ground water can be determined if other considerations indicate approval.
- ☐ 11. If restroom facilities are to be installed, then a sewage system MUST be installed to meet Idaho State Sewage Regulations.
- ☐ 12. We will require plans be submitted for a plan review for any:  
☐ food establishment ☐ swimming pools or spas ☐ child care center  
☐ beverage establishment ☐ grocery store
- ☒ 13. Infiltration beds for storm water disposal are considered shallow injection wells. An application and fee must be submitted to CDH.
- ☐ 14. \_\_\_\_\_

Reviewed By: Lois Bodey

Date: 6/10/2021



# Communities in Motion 2040 2.0 Development Review

The Community Planning Association of Southwest Idaho (COMPASS) is the metropolitan planning organization (MPO) for Ada and Canyon Counties. COMPASS has developed this review as a tool for local governments to evaluate whether land developments are consistent with the goals of *Communities in Motion 2040 2.0* (CIM 2040), the regional long-range transportation plan for Ada and Canyon Counties. This checklist is not intended to be prescriptive, but rather a guidance document based on CIM 2040 2.0 goals.

**Development Name: Fountain Park**






**Agency: Star**

**CIM Vision Category: Future Neighborhoods**

**New households: 251**

**New jobs: 0**

**Exceeds CIM forecast: Yes**

|  |   |   |
|--|---|---|
|    | CIM Corridor: <b>None</b><br>Pedestrian level of stress: <b>N/A</b><br>Bicycle level of stress: <b>N/A</b>  | Level of Stress considers facility type, number of vehicle lanes, and speed. Roads with <b>G or PG</b> ratings better support bicyclists and pedestrians of all ages and comfort levels.  |
|    | Housing within 1 mile: <b>690</b><br>Jobs within 1 mile: <b>130</b><br>Jobs/Housing Ratio: <b>0.2</b>   | A good jobs/housing balance – a ratio between <b>1 and 1.5</b> – reduces traffic congestion. Higher numbers indicate the need for more housing and lower numbers indicate an employment need.                                   |
|   | Nearest police station: <b>2.6 miles</b><br>Nearest fire station: <b>2.5 miles</b>  | Developments within <b>1.5 miles</b> of police and fire stations ensure that emergency services are more efficient and reduce the cost of these important public services.  |
|  | Farmland consumed: <b>Yes</b><br>Farmland within 1 mile: <b>802</b>   | Farmland contributes to the local economy, creates additional jobs, and provides food security to the region. Development in farm areas decreases the productivity and sustainability of farmland.                              |
|  | Nearest bus stop: <b>&gt;4 miles</b><br>Nearest public school: <b>2.4 miles</b><br>Nearest public park: <b>1.3 miles</b><br>Nearest grocery store: <b>&gt;4 miles</b> | Residents who live or work less than <b>½ mile</b> from critical services have more transportation choices. Walking and biking reduces congestion by taking cars off the road, while supporting a healthy and active lifestyle. |

## Recommendations

This proposal exceeds growth forecasted for this area. Transportation infrastructure may not be able to support the new transportation demands.

COMPASS has identified the installation of a traffic signal at the State Highway 44 (State Street) and Palmer Lane intersection (Key 22718) in the FY2021-2027 Regional Transportation Improvement Program. Construction is scheduled for 2027.

ValleyConnect 2.0 proposes a regional bus route from Canyon County to downtown Meridian via Highway 44. This service would be less than ½ a mile from this development when operational.

More information about COMPASS and *Communities in Motion 2040 2.0*:

Web: [www.compassidaho.org](http://www.compassidaho.org)

Email [info@compassidaho.org](mailto:info@compassidaho.org)

More information about the development review process:

<http://www.compassidaho.org/dashboard/devreview.htm>

## SH-44 (State Street) and SH-55 (Eagle Road) Intersection, 1/2 CFI, Eagle

Regionally Significant: ☐ ☒ Inflated

Key #: 13476

Requesting Agency: ITD

Project Year: 2021

Total Previous Expenditures: \$1,376

Total Programmed Cost: \$7,980

Total Cost (Prev. + Prog.): \$9,356

**Project Description :** Construct a partial (1/2) continuous flow intersection (CFI), including displaced left-turn lanes, at State Highway 44 (State Street) and State Highway 55 (Eagle Road) in the City of Eagle.

### COMPASS PM:

Congestion Reduction/System Reliability  
Freight Movement and Economic Vitality  
Transportation Safety  
Community Infrastructure

### Federal PM:



| Funding Source NHPP |                         |                                    | Program State Hwy - Safety & Capacity (Capacity) |            |                          |                |                | Local Match 7.34% |              |
|---------------------|-------------------------|------------------------------------|--|------------|--------------------------|----------------|----------------|-------------------|--------------|
| Cost Year*          | Preliminary Engineering | Preliminary Engineering Consulting | Right-of-Way                                     | Utilities  | Construction Engineering | Construction   | Total          | Federal Share     | Local Share  |
| 2021                | 30                      | 326                                | 0  | 0          | 947                      | 6,677          | 7,980          | 7,394             | 586          |
| <b>Fund Totals:</b> | <b>\$30</b>             | <b>\$326</b>                       | <b>\$0</b>                                       | <b>\$0</b> | <b>\$947</b>             | <b>\$6,677</b> | <b>\$7,980</b> | <b>\$7,394</b>    | <b>\$586</b> |

## SH-44 (State Street), Palmer Lane Intersection Improvements, Star

Regionally Significant: ☐ ☐ Inflated

Key #: 22718

Requesting Agency: Private Developer

Project Year: 2027

Total Previous Expenditures: \$0

Total Programmed Cost: \$850

Total Cost (Prev. + Prog.): \$850

**Project Description :** Install a traffic signal at the State Highway 44 (State Street) and Palmer Lane intersection in the City of Star to improve safety and mobility.

### COMPASS PM:

Congestion Reduction/System Reliability  
Transportation Safety  
Community Infrastructure

### Federal PM:



| Funding Source Local (Regionally Significant) |                         |                                    | Program Hwy - Local Partnerships |            |                          |              |              | Local Match 100.00% |              |
|---|-------------------------|------------------------------------|----------------------------------|------------|--------------------------|--------------|--------------|---------------------|--------------|
| Cost Year*                                    | Preliminary Engineering | Preliminary Engineering Consulting | Right-of-Way                     | Utilities  | Construction Engineering | Construction | Total        | Federal Share       | Local Share  |
| 2021  | 70                      | 0                                  | 0                                | 0          | 0                        | 0            | 70           | 0                   | 70           |
| 2027  | 0                       | 0                                  | 0                                | 0          | 85                       | 695          | 780          | 0                   | 780          |
| <b>Fund Totals:</b>                           | <b>\$70</b>             | <b>\$0</b>                         | <b>\$0</b>                       | <b>\$0</b> | <b>\$85</b>              | <b>\$695</b> | <b>\$850</b> | <b>\$0</b>          | <b>\$850</b> |



**Project/File:** **Fountain Park Subdivision/ SPP21-0015/ AZ-21-19/ DA-21-24/ PP-21-17**  
*This is an annexation and rezone application to annex 60.21 acres into the City of Star with an R-5 zoning and a preliminary plat application to develop 251 residential lots and 27 common lots on 60.21 acres with a development agreement.*

**Lead Agency:** City of Star

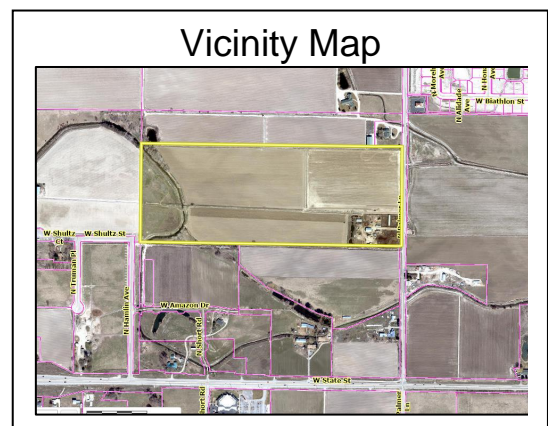
**Site address:** 621 N. Palmer Lane

**Staff Approval:** August 6, 2021

**Applicant:** Challenger Development Inc.  
 1977 E. Overland Road  
 Meridian, ID 83642

**Representative:** Becky McKay  
 Engineering Solutions  
 1029 N. Rosario Street, Suite 100  
 Meridian, ID 83642

**Staff Contact:** Paige Bankhead, E.I.  
 Phone: 387-6293  
 E-mail: [pbankhead@achdidaho.org](mailto:pbankhead@achdidaho.org)



## A. Findings of Fact

- Description of Application:** The applicant is requesting approval of an annexation and rezone application to annex 60.21 acres into the City of Star with an R-5 zoning and a preliminary plat application to develop 251 residential lots and 27 common lots on 60.21 acres with a development agreement.

The City of Star designates this area as Neighborhood Residential.

- Description of Adjacent Surrounding Area:**

| Direction | Land Use   | Zoning   |
|-----------|--|----------|
| North     | Rural Urban Transition (Ada County)                          | RUT      |
| South     | High Density Residential/Rural Urban Transition (Ada County) | R-13/RUT |
| East      | Rural Urban Transition (Ada County)                          | RUT      |
| West      | Rural Urban Transition (Ada County)                          | RUT      |

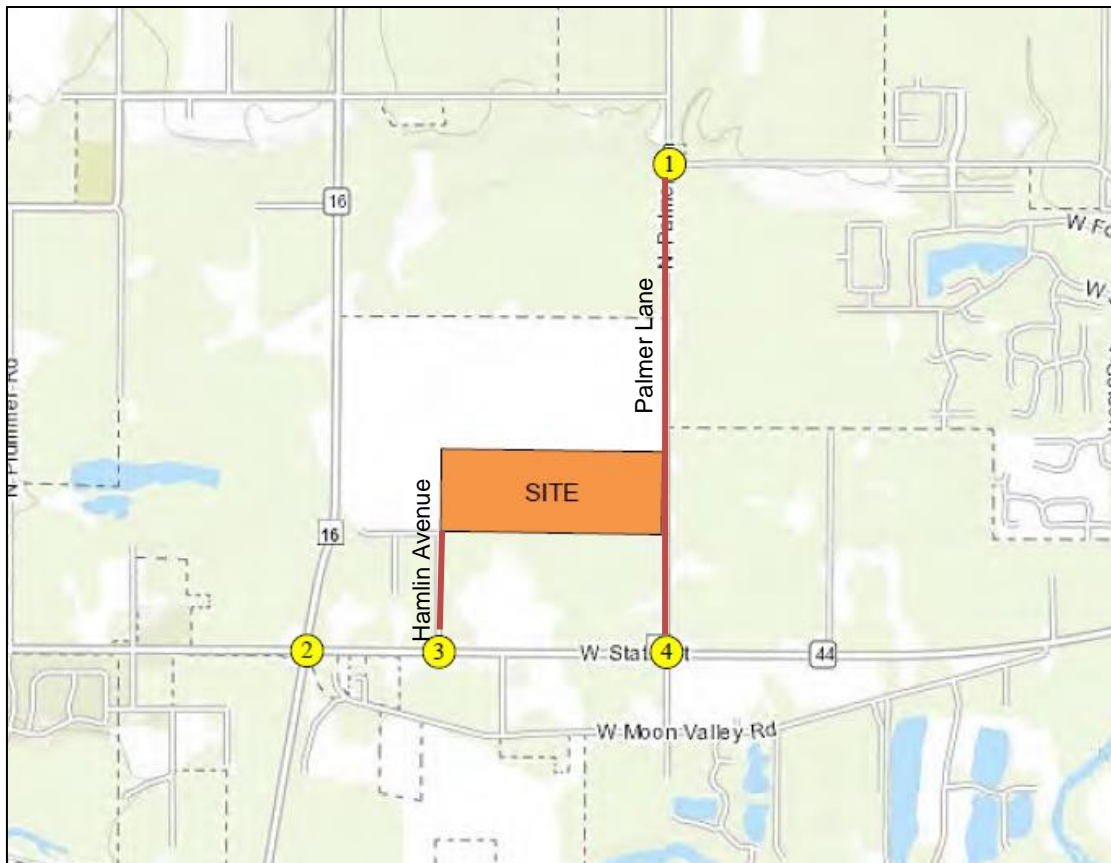
- Site History:** ACHD has not previously reviewed this site for a development application.
- Transit:** Transit services are not available to serve this site.
- New Center Lane Miles:** The proposed development includes 1.99 centerline miles of new public road.



6. **Impact Fees:** There will be an impact fee that is assessed and due prior to issuance of any building permits. The assessed impact fee will be based on the impact fee ordinance that is in effect at that time. The impact fee assessment will not be released until the civil plans are approved by ACHD.
7. **Capital Improvements Plan (CIP)/ Integrated Five Year Work Plan (IFYWP):**  
There are no roadways, bridges or intersections in the general vicinity of the project that are in the Integrated Five Year Work Plan (IFYWP) or the District's Capital Improvement Plan (CIP).
8. **Roadways to Bikeways Master Plan:** ACHD's Roadways to Bikeways Master Plan (BMP) was adopted by the ACHD Commission in May of 2009 and was update in 2018. The plan seeks to implement the Planned Bicycle Network to support bicycling as a viable transportation option for Ada County residents with a wide range of ages and abilities, maintain bicycle routes in a state of good repair in order to ensure they are consistently available for use, promote awareness of existing bicycle routes and features and support encouragement programs and to facilitate coordination and cooperation among local jurisdictions in implementing the Roadways to Bikeways Plan recommendations.
  - The BMP identifies Palmer Lane Roads as Level 2 facility. The applicant will be required to improve Palmer Lane abutting the site as ½ of a 36-foot wide collector street section with curb, gutter, sidewalk and pavement for bike lanes.

## **B. Traffic Findings for Consideration**

1. **Trip Generation:** This development is estimated to generate 2,271 additional vehicle trips per day; 219 additional vehicle trips per hour in the PM peak hour, based on the traffic impact study.
2. **Traffic Impact Study**  
CR Engineering, Inc. prepared a traffic impact study for the proposed Fountain Park Subdivision. An executive summary of the findings **as presented by CR Engineering, Inc is in Attachment 3.** The following executive summary is **not the opinion of ACHD staff**. ACHD has reviewed the submitted traffic impact study for consistency with ACHD policies and practices, and may have additional requirements beyond what is noted in the summary. ACHD Staff comments on the submitted traffic impact study can be found below under staff comments.



Intersections (yellow) and roadway segments (red) included in the study.

#### a. Policy

**Mitigation Proposals:** Mitigation recommendations shall be provided within the report. At a minimum, for each roadway segment and intersection that does not meet the minimum acceptable level of service planning threshold or v/c ratio, the report must discuss feasible measures to avoid or reduce the impact to the system. To be considered adequate, measures should be specific and feasible. Mitigation may also include:

- Revision to the Phasing Plan to coincide with the District's planning Capital Projects.
- Reducing the scope and/or scale of the project.

**Alternative Mitigation Measures:** 7106.7.3 states that if traditional mitigation measures such as roadway widening and intersection improvements are infeasible as determined by ACHD, the TIS may recommend alternative mitigation measures. Alternative mitigation measures shall demonstrate that impacts from the project will be offset.

- If the impacted roadway segments and/or intersections are programmed as funded in the Integrated Five Year Work Plan (IFYWP) or the Capital Improvements Plan (CIP); no alternative mitigation is required.
- If the impacted roadway segments and/or intersections are not programmed in either the IFYWP or the CIP; the applicant may (i) analyze the shoulder hour and (ii) provide a safety analysis to determine alternative mitigation requirements.
  - If the impacted roadway segments and intersections meet the minimum acceptable level of service planning thresholds in the shoulder hour the applicant may suggest feasible alternative mitigation such as: sidewalks, bike facilities, connectivity, safety improvements, etc. within 1.5 miles of the proposed development.

- If the shoulder hour planning thresholds are exceeded the applicant may request to enter into a Development Agreement and pay into the Priority Corridor Fund an amount determined by the ACHD to offset impacts from the project.
- Alternative Mitigation may also include:
  - Revision to the Phasing Plan to coincide with the District's future Capital Projects.
  - Reducing the scope and/or scale of the project.

**Level of Service Planning Thresholds:** District Policy 7206.4.1 states that, Level of Service Planning Thresholds have been established for principal arterials and minor arterials within ACHD's Capital Improvement Plan and are also listed in section 7106. Unless otherwise required to provide a Traffic Impact Study under section 7106, a proposed development with site traffic less than 10% of the existing downstream roadway or intersection peak hour traffic shall not be required to provide mitigation for a roadway or intersection that currently exceeds the minimum acceptable level of service planning threshold or V/C ratio.

- b. Staff Comments/Recommendations:** Staff has reviewed the submitted traffic impact study (TIS) and generally agrees with the findings and recommendations. The study found that all roadway segments meet ACHD's Acceptable Level of Service (LOS) thresholds under the existing, 2025 background and 2025 total traffic conditions. The study does not recommend any additional dedicated turn lanes at the site access proposed on Palmer Lane.

The study recommends signaling the intersection of SH-44/Palmer Lane and constructing a dedicated southbound left-turn lane and northbound left-turn at the intersection lane under the 2025 background conditions and total conditions. The Idaho Transportation Department (ITD) has programmed this intersection to be signalized in 2027 and has requested that the applicant contribute their proportionate share to the future improvements at the intersection of SH-44/Palmer Lane. The applicant should not be required to construct the improvements recommended on the northbound and southbound legs be constructed with this development due to the fact that the peak hour site trips at this intersection are 5.4% in the PM peak hour, less than the 10% threshold to require improvements per District Policy 7206.4.1.

The study shows that the intersection of Hamlin Avenue/SH-44 operates acceptably under all traffic conditions. This intersection is currently restricted to right-in/right-out only.

The study recommends that Wilder Farms Street, the public road proposed to provide access to the subdivision on Palmer Lane, be classified as a collector street section due to the proposed average daily traffic (ADT) of 1,960 trips with 129 trips in the PM peak hour at the total build out of the development. Staff supports the applicant's proposal to construct this road as a collector street.

### 3. Condition of Area Roadways

Traffic Count is based on Vehicles per hour (VPH)

| Roadway         | Frontage | Functional Classification | PM Peak Hour Traffic Count | PM Peak Hour Level of Service | Existing Plus Project |
|-----------------|----------|---------------------------|----------------------------|-------------------------------|-----------------------|
| Palmer Lane     | 990-feet | Collector                 | 76                         | Better than "D"               | Better than "D"       |
| **Hamlin Avenue | 0-feet   | Local                     | 1                          | NA                            | NA                    |

\* Acceptable level of service for a two-lane principal arterial is "E" (690 VPH).



\* Acceptable level of service for a two-lane collector is “D” (425 VPH).

\*\* ACHD does not set level of service thresholds for local roadways.

#### 4. **Average Daily Traffic Count (VDT)**

*Average daily traffic counts are based on ACHD’s most current traffic counts.*

- The average daily traffic count for Palmer Lane north of SH-44 was 591 on 04/04/2018.
- The average daily traffic count for Hamlin Avenue north of SH-44 was 8 on 12/11/2019.

## **C. Findings for Consideration**

### **1. Palmer Lane – Collector Road**

- a. **Existing Conditions:** Palmer Lane is improved with 2-travel lanes and no curb, gutter or sidewalk abutting the site. There is 50-feet of right-of-way for Palmer Lane (20-feet from centerline).

b. **Policy:**

**Collector Street Policy:** District policy 7206.2.1 states that the developer is responsible for improving all collector frontages adjacent to the site or internal to the development as required below, regardless of whether access is taken to all of the adjacent streets.

**Master Street Map and Typologies Policy:** District policy 7206.5 states that if the collector street is designated with a typology on the Master Street Map, that typology shall be considered for the required street improvements. If there is no typology listed in the Master Street Map, then standard street sections shall serve as the default.

**Street Section and Right-of-Way Policy:** District policy 7206.5.2 states that the standard right-of-way width for collector streets shall typically be 50 to 70-feet, depending on the location and width of the sidewalk and the location and use of the roadway. The right-of-way width may be reduced, with District approval, if the sidewalk is located within an easement; in which case the District will require a minimum right-of-way width that extends 2-feet behind the back-of-curb on each side.

The standard street section shall be 46-feet (back-of-curb to back-of-curb). This width typically accommodates a single travel lane in each direction, a continuous center left-turn lane, and bike lanes.

**Residential Collector Policy:** District policy 7206.5.2 states that the standard street section for a collector in a residential area shall be 36-feet (back-of-curb to back-of-curb). The District will consider a 33-foot or 29-foot street section with written fire department approval and taking into consideration the needs of the adjacent land use, the projected volumes, the need for bicycle lanes, and on-street parking.

**Off-Site Streets Policy:** District Policy 7206.2.3 states that if the proposed development is not served by a public street that is fully improved to urban standards (curb, gutter, sidewalk) or a minimum 30-feet of pavement, then the developer shall provide 30-feet of pavement with 3-foot wide gravel shoulders from the site to the public street specified by the District; OR the developer shall provide 24-feet of pavement with 3-foot wide gravel shoulders and a minimum 6-foot wide detached asphalt/concrete pedestrian facility, from the site to a public street specified by the District.

Alternatives to pavement widening including sidewalks and pathways or other proposals, may be considered by the District. The extent of roadway improvements (improvement type and length) will be determined by evaluating certain criteria. Criteria to establish improvement type and length include but are limited to: traffic volumes (existing and projected); number of pedestrians (existing and projected); location of pedestrian “attractors” and “generators” (i.e. parks and schools); number of access points/streets serving the proposed development; usable

right-of-way; need for traffic calming; utilities and irrigation facilities. All utility relocation costs associated with the off-site street widening shall be borne by the developer.

**Sidewalk Policy:** District policy 7206.5.6 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all collector streets. A parkway strip at least 6-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip. Sidewalks constructed next to the back-of-curb shall be a minimum of 7-feet wide.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**ACHD Master Street Map:** ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, collector street requirements, and specific roadway features required through development. This segment of Palmer Lane is designated in the MSM as a Residential Collector with 2-lanes and on-street bike lanes, a 36-foot street section within 50-feet of right-of-way.

- c. **Applicant Proposal:** The applicant has proposed to improve Palmer Lane abutting the site as ½ of a 36-foot wide street section with vertical curb, gutter, and 5-foot wide detached sidewalk and dedicate additional right-of-way to total 37-feet from the centerline of Palmer Lane abutting the site.
- d. **Staff Comments/Recommendations:** The applicant's proposal meets District Policy and should be approved, as proposed. If street trees are desired, an 8-foot wide planter strip is required. For detached sidewalk, the applicant may reduce the right-of-way to 2-feet behind the back of curb and provide a right-of-way easement that extends from the right-of-way line to 2-feet behind the back of curb.

Palmer Lane from the site to SH-44 has approximately 25-feet of pavement. District Policy requires that if a proposed development is not served by a public street that is fully improved to urban standards (curb, gutter, sidewalk) or a minimum 30-feet of pavement, then the developer shall provide 30-feet of pavement with 3-foot wide gravel shoulders from the site to the public street specified by the District; OR the developer shall provide 24-feet of pavement with 3-foot wide gravel shoulders and a minimum 6-foot wide detached asphalt/concrete pedestrian facility, from the site to a public street specified by the District. Therefore, the applicant should be required to widen the pavement to 30-feet from the site's south property line to SH-44 or provide 24-feet of pavement with 3-foot wide gravel shoulders and a minimum 6-foot wide detached asphalt/concrete pedestrian facility from the site's south property line to SH-44.

## 2. New N/S Collector Roadway - Wilder Farms Drive/Tallis Avenue

- a. **Existing Conditions:** There are no existing roadways within the site.
- b. **Policy:**
  - Collector Street Policy:** District policy 7206.2.1 states that the developer is responsible for improving all collector frontages adjacent to the site or internal to the development as required below, regardless of whether access is taken to all of the adjacent streets.
  - Master Street Map and Typologies Policy:** District policy 7206.5 states that if the collector street is designated with a typology on the Master Street Map, that typology shall be considered

for the required street improvements. If there is no typology listed in the Master Street Map, then standard street sections shall serve as the default.

**Street Section and Right-of-Way Policy:** District policy 7206.5.2 states that the standard right-of-way width for collector streets shall typically be 50 to 70-feet, depending on the location and width of the sidewalk and the location and use of the roadway. The right-of-way width may be reduced, with District approval, if the sidewalk is located within an easement; in which case the District will require a minimum right-of-way width that extends 2-feet behind the back-of-curb on each side.

The standard street section shall be 46-feet (back-of-curb to back-of-curb). This width typically accommodates a single travel lane in each direction, a continuous center left-turn lane, and bike lanes.

**Residential Collector Policy:** District policy 7206.5.2 states that the standard street section for a collector in a residential area shall be 36-feet (back-of-curb to back-of-curb). The District will consider a 33-foot or 29-foot street section with written fire department approval and taking into consideration the needs of the adjacent land use, the projected volumes, the need for bicycle lanes, and on-street parking.

**Sidewalk Policy:** District policy 7206.5.6 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all collector streets. A parkway strip at least 6-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip. Sidewalks constructed next to the back-of-curb shall be a minimum of 7-feet wide.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**ACHD Master Street Map:** ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, collector street requirements, and specific roadway features required through development. A new collector roadway was identified on the MSM with the street typology of Commercial Collector. The new collector roadway should extend from the site's south property line through the property stubbing to the north. The Commercial Collector typology as depicted in the Livable Street Design Guide recommends a 3-lane roadway with bike lanes, and on street parking, a 46-foot street section within 70-feet of right-of-way.

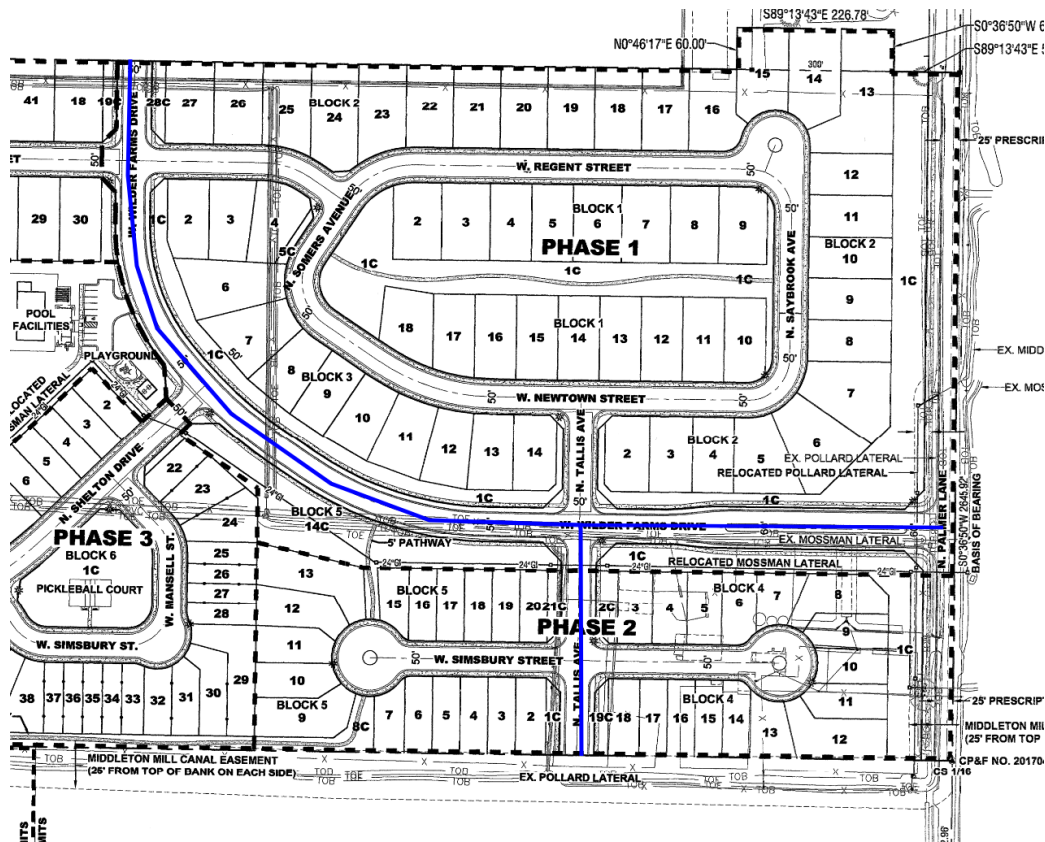
**Landscape Medians Policy:** District policy 7207.5.16 states that landscape medians are permissible where adequate pavement width is provided on each side of the median to accommodate the travel lanes and where the following is provided:

- The median is platted as right-of-way owned by ACHD.
- The width of an island near an intersection is 12-feet maximum for a minimum distance of 150-feet. Beyond the 150-feet, the island may increase to a maximum width of 30-feet.
- At an intersection that is signalized or is to be signalized in the future, the median width shall be reduced to accommodate the necessary turn lane storage and tapers.
- The Developer or Homeowners Association shall apply for a license agreement if landscaping is to be placed within these medians.



- The license agreement shall contain the District's requirements of the developer including, but not limited to, a "hold harmless" clause; requirements for maintenance by the developer; liability insurance requirements; and restrictions.
- Vertical curbs are required around the perimeter of any raised median. Gutters shall slope away from the curb to prevent ponding.

c. **Applicant Proposal:** The applicant has proposed to construct one collector roadway, Wilder Farms Drive, to intersect Palmer Lane and extend it through the site to stub to the site's north property line. The applicant has proposed to construct Tallis Avenue to stub to the site's south property line and intersect Wilder Farms Drive.



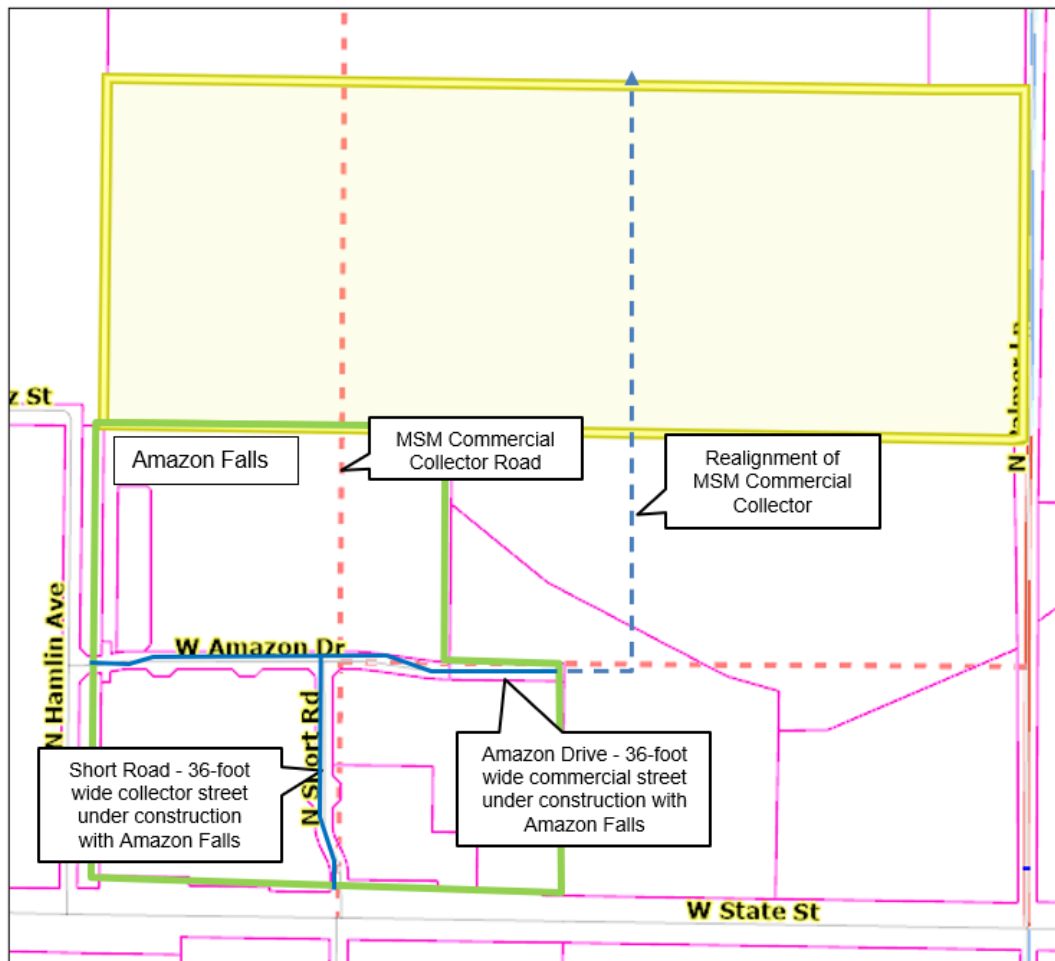
Collector road network within the site in blue

The applicant has proposed to construct the streets as 36-foot wide collector street sections with vertical curb, gutter, 8-foot wide planter strips within 50-feet of right-of-way and 5-foot wide detached concrete sidewalks located outside of the right-of-way. The applicant has proposed to construct the entry portion of Wilder Farms Drive as a 52-foot wide street section with 21-foot wide travel lanes, a 12-foot wide landscape median, vertical curb, gutter, 8-foot wide planter strips within 60-feet of right-of-way and 5-foot wide detached concrete sidewalks located outside of the right-of-way.

d. **Staff Comments/Recommendations:** The applicant's proposal to construct Wilder Farms Drive and Tallis Avenue meets District Policy and should be approved, as proposed. For detached sidewalk located outside of the right-of-way, the applicant should be required to provide a permanent right-of-way easement that extends from the right-of-way line to 2-feet behind the back of sidewalk.

The applicant should be required to plat the landscape median as right-of-way owned by ACHD. The applicant or homeowners association shall enter into a license agreement with ACHD for the maintenance of the landscape median.

The north/south collector shown on the MSM through the site has been shifted to the east since it cannot be constructed through the Amazon Falls development as that development application was submitted prior to ACHD's acceptance of the Master Street Map that includes the north/south commercial collector through that site.



### 3. Off-site Hamlin Avenue – Collector

#### a. Policy:

**Collector Street Policy:** District policy 7206.2.1 states that the developer is responsible for improving all collector frontages adjacent to the site or internal to the development as required below, regardless of whether access is taken to all of the adjacent streets.

**Master Street Map and Typologies Policy:** District policy 7206.5 states that if the collector street is designated with a typology on the Master Street Map, that typology shall be considered for the required street improvements. If there is no typology listed in the Master Street Map, then standard street sections shall serve as the default.

**Street Section and Right-of-Way Policy:** District policy 7206.5.2 states that the standard right-of-way width for collector streets shall typically be 50 to 70-feet, depending on the location and width of the sidewalk and the location and use of the roadway. The right-of-way width may be reduced, with District approval, if the sidewalk is located within an easement; in which case the District will require a minimum right-of-way width that extends 2-feet behind the back-of-curb on each side.

The standard street section shall be 46-feet (back-of-curb to back-of-curb). This width typically accommodates a single travel lane in each direction, a continuous center left-turn lane, and bike lanes.

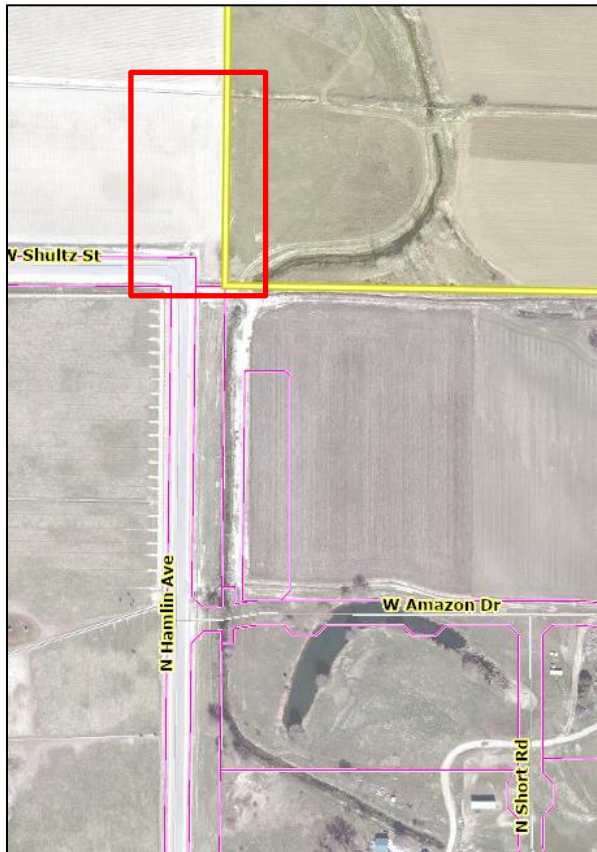
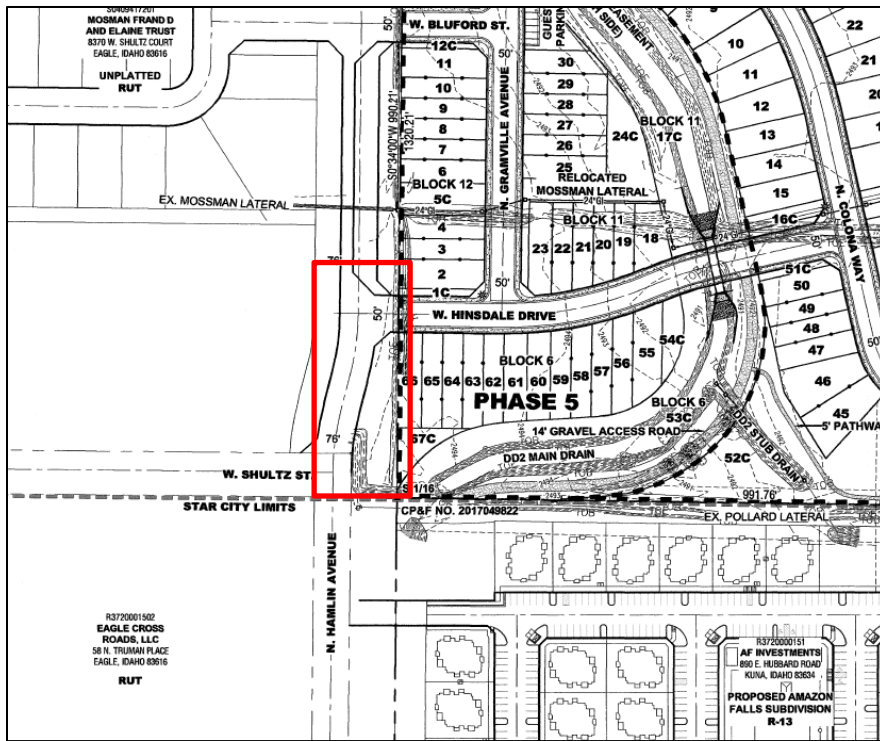
**Residential Collector Policy:** District policy 7206.5.2 states that the standard street section for a collector in a residential area shall be 36-feet (back-of-curb to back-of-curb). The District will consider a 33-foot or 29-foot street section with written fire department approval and taking into consideration the needs of the adjacent land use, the projected volumes, the need for bicycle lanes, and on-street parking.

**Off-Site Streets Policy:** District Policy 7206.2.3 states that if the proposed development is not served by a public street that is fully improved to urban standards (curb, gutter, sidewalk) or a minimum 30-feet of pavement, then the developer shall provide 30-feet of pavement with 3-foot wide gravel shoulders from the site to the public street specified by the District; OR the developer shall provide 24-feet of pavement with 3-foot wide gravel shoulders and a minimum 6-foot wide detached asphalt/concrete pedestrian facility, from the site to a public street specified by the District.

Alternatives to pavement widening including sidewalks and pathways or other proposals, may be considered by the District. The extent of roadway improvements (improvement type and length) will be determined by evaluating certain criteria. Criteria to establish improvement type and length include but are limited to: traffic volumes (existing and projected); number of pedestrians (existing and projected); location of pedestrian “attractors” and “generators” (i.e. parks and schools); number of access points/streets serving the proposed development; usable right-of-way; need for traffic calming; utilities and irrigation facilities. All utility relocation costs associated with the off-site street widening shall be borne by the developer.

- b. **Applicant Proposal:** The applicant has proposed to extend Hamlin Avenue located off-site to the southwest, shown below, to the north to intersect with Hinsdale Drive, a local road. The applicant has proposed to dedicate 76-feet of right-of-way for Hamlin Avenue, but has not proposed a street section for this off-site road segment. They have indicated they are currently working with that property owner to obtain right-of-way or a permanent right-of-way easement to construct Hamlin Avenue and Hinsdale Avenue.





- c. **Staff Comments/Recommendations:** In order to construct Hamlin Avenue off-site as proposed, the applicant should be required to obtain approval from the property owner directly west of the site, and provide the legal description and exhibit for the right-of-way dedication to ACHD for the off-site segments prior to ACHD's approval of the construction plans that include the construction of Hinsdale Drive to the site's west property line. The applicant should be required to construct

Hamlin Avenue off-site in the alignment proposed from Schultz Street to Hinsdale Drive with a minimum of 30-feet of pavement and 3-foot wide gravel shoulders, or provide 24-feet of pavement with 3-foot wide gravel shoulders and a minimum 6-foot wide detached asphalt/concrete pedestrian facilities. The applicant may construct Hamlin Avenue off-site as a 36-foot wide collector street section with vertical curb, gutter and 5-foot wide detached or 7-foot wide attached concrete sidewalk if desired. The applicant's proposal to dedicate 76-feet of right-of-way for the off-site segment of Hamlin Avenue will accommodate a 36-foot wide collector street section with sidewalks and should be approved, as proposed.

If street trees are desired, an 8-foot wide planter strip should be required.

For detached sidewalk located outside of the right-of-way, the applicant may reduce the right-of-way width to 2-feet behind the back of sidewalk and provide a permanent right-of-way easement that extends from the right-of-way line to 2-feet behind the back of sidewalk.

This off-site segment of Hamlin Avenue will be required to be fully improved as a 36-foot wide collector street section with the development of that parcel in the future.

#### **4. Internal Local Roads and Off-site Hinsdale Drive**

**a. Existing Conditions:** There are no existing local roads within the site.

**b. Policy:**

**Local Roadway Policy:** District Policy 7207.2.1 states that the developer is responsible for improving all local street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

**Street Section and Right-of-Way Policy:** District Policy 7207.5 states that right-of-way widths for all local streets shall generally not be less than 47-feet wide and that the standard street section shall be 33-feet (back-of-curb to back-of-curb).

**Standard Urban Local Street—33-foot Street Section and Right-of-way Policy:** District Policy 7207.5.2 states that the standard street section shall be 33-feet (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size. This street section shall include curb, gutter, and minimum 5-foot wide concrete sidewalks on both sides and shall typically be constructed within 47-feet of right-of-way.

For the City of Kuna and City of Star: Unless otherwise approved by Kuna or Star, the standard street section shall be 36-feet (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size. This street section shall include curb, gutter, and minimum 5-foot wide concrete sidewalks on both sides and shall typically be constructed within 50-feet of right-of-way.

**Continuation of Streets Policy:** District Policy 7207.2.4 states that an existing street, or a street in an approved preliminary plat, which ends at a boundary of a proposed development shall be extended in that development. The extension shall include provisions for continuation of storm drainage facilities. Benefits of connectivity include but are not limited to the following:

- Reduces vehicle miles traveled.
- Increases pedestrian and bicycle connectivity.
- Increases access for emergency services.
- Reduces need for additional access points to the arterial street system.
- Promotes the efficient delivery of services including trash, mail and deliveries.
- Promotes appropriate intra-neighborhood traffic circulation to schools, parks, neighborhood commercial centers, transit stops, etc.
- Promotes orderly development.

**Sidewalk Policy:** District Policy 7207.5.7 states that five-foot wide concrete sidewalk is required on both sides of all local street, except those in rural developments with net densities of one dwelling unit per 1.0 acre or less, or in hillside conditions where there is no direct lot frontage, in which case a sidewalk shall be constructed along one side of the street. Some local jurisdictions may require wider sidewalks.

The sidewalk may be placed next to the back-of-curb. Where feasible, a parkway strip at least 8-feet wide between the back-of-curb and the street edge of the sidewalk is recommended to provide increased safety and protection of pedestrians and to allow for the planting of trees in accordance with the District's Tree Planting Policy. If no trees are to be planted in the parkway strip, the applicant may submit a request to the District, with justification, to reduce the width of the parkway strip.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**Cul-de-sac Streets Policy:** District policy 7207.5.8 requires cul-de-sacs to be constructed to provide a minimum turning radius of 50-feet; in rural areas or for temporary cul-de-sacs the emergency service providers may require a greater radius. Landscape and parking islands may be constructed in turnarounds if a minimum 29-foot street section is constructed around the island. The pavement width shall be sufficient to allow the turning around of a standard AASHTO SU design vehicle without backing. The developer shall provide written approval from the appropriate fire department for this design element.

The District will consider alternatives to the standard cul-de-sac turnaround on a case-by-case basis. This will be based on turning area, drainage, maintenance considerations and the written approval of the agency providing emergency fire service for the area where the development is located.

**Off-Site Streets Policy:**

Local Streets with less than 400 VTD (existing + proposed)

District Policy 7207.2.3 states that if the proposed development is not served by a public street with at least 24-feet of pavement then the developer shall pave the street or widen the existing pavement to provide 24-feet of pavement with 3-foot gravel shoulders from the site to a public street specified by the District.

Local Streets with 400 to 1,000 VTD (existing or proposed)

District Policy 7207.2.3 states that if the proposed development is not served by a public street with at least 30-feet of pavement then the developer shall pave the street or widen the existing pavement to provide a minimum 24 to 30-feet of pavement as determined by the District, with 3-foot gravel shoulders from the site to a public street specified by the District.

Local Streets with greater than 1,000 VTD (existing or proposed)

District Policy 7207.2.3 states that if the proposed development is not served by a public street with a minimum of 30-feet of pavement, then the developer shall pave the street or widen the existing pavement to provide 30-feet of pavement with 3-foot gravel shoulders from the site to a public street specified by the District; OR shall provide 24-feet of pavement with 3-foot gravel shoulders and a minimum 6-foot wide detached asphalt/concrete pedestrian facility from the site to a public street specified by the District.

All Local Streets

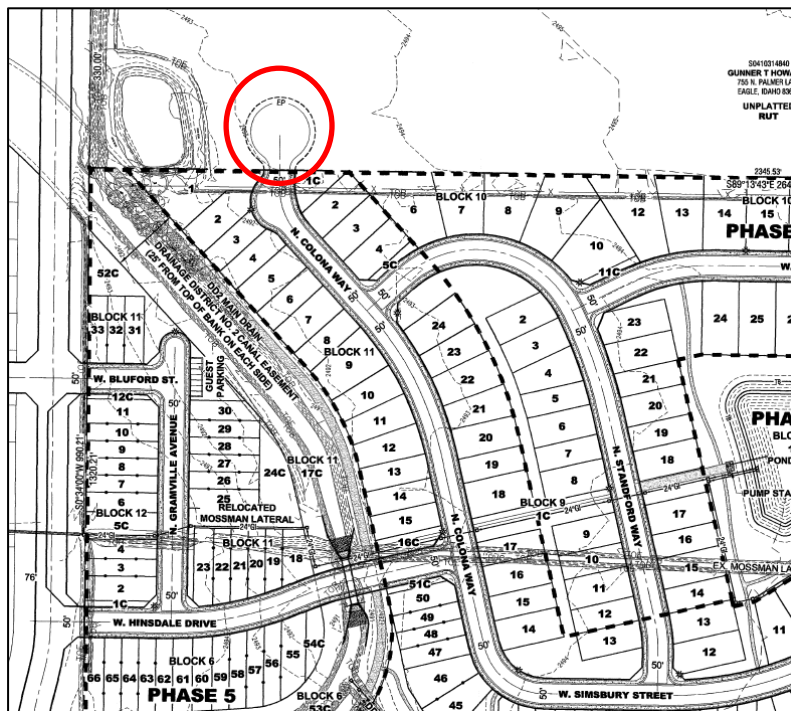
Alternatives to pavement widening include sidewalks, pathways, or other proposals such as

passive traffic calming measures or mitigation through design elements, may be considered by the District. The extend of the roadway improvements (improvement type and length) will be determined by evaluating site specific criteria. Criteria to establish improvement type and length include but are not limited to: traffic volumes (existing and projected); number of pedestrians (existing and projected); location of pedestrian “attractors” and “generators” (i.e. parks and schools); number of access points/streets serving the proposed development; usable right-of-way; need for traffic calming; utilities and irrigation facilities. All utility relocation costs associated with the off-site street widening shall be borne by the developer.

- c. Applicant’s Proposal:** The applicant has proposed to construct all internal local roads as 36-foot wide local street sections with curb, gutter and 5-foot wide concrete sidewalk within 50-feet of right-of-way.

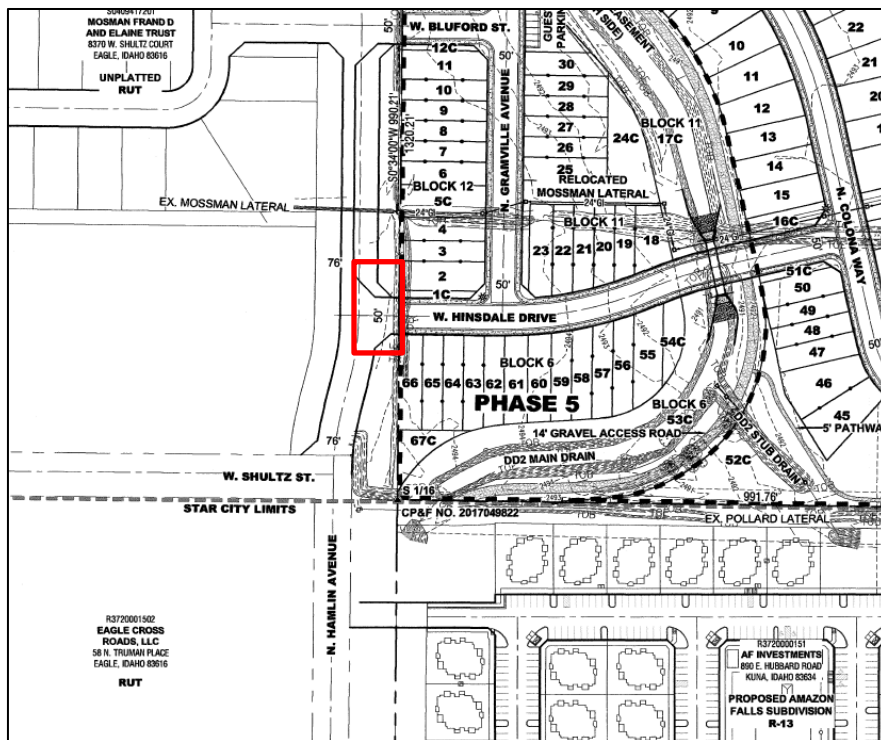
The applicant has proposed to construct 5 knuckles, 3 with landscape islands. The applicant has also proposed to construct 2 cul-de-sacs with landscape islands.

The applicant has proposed to construct a cul-de-sac off-site at the terminus of Colona Way, a local road that stubs to the site’s north property line, shown below.



The applicant has proposed to construct a small portion of Hinsdale Drive to intersect with Hamlin Avenue off-site and dedicate 50-feet of right-of-way for this small off-site segment shown below, but has not proposed a street section for this segment. They have indicated they are currently working with that property owner to obtain right-of-way or a permanent right-of-way easement to construct Hamlin Avenue and Hinsdale Avenue.





**Staff Comments/Recommendations:** The applicant's proposal for the internal local road street sections, cul de sacs and knuckles meets District Policy and should be approved, as proposed. The applicant shall plat the landscape islands as right-of-way owned by ACHD. The applicant or homeowners association shall enter into a license agreement with ACHD for the maintenance of the landscape islands.

In order to construct Hinsdale Drive to intersect Hamlin Avenue off-site as proposed, the applicant should be required to obtain approval from the property owner directly west of the site, and provide the legal description and exhibit for the right-of-way dedication to ACHD for the off-site segments prior to ACHD's approval of the construction plans that include the construction of Hinsdale Drive to the site's west property line. At a minimum, the applicant should be required to construct the off-site segment of Hinsdale Drive with a minimum of 24-feet of pavement with 3-foot wide gravel shoulders within a minimum of 50-feet of right-of-way. Hinsdale Drive will be required to be fully improved as a full 36-foot wide local street section with curb, gutter and sidewalk within 50-feet of right-of-way when that parcel develops in the future. The applicant may construct this road as a 36-foot wide local street section with curb, gutter and 5-foot wide concrete sidewalk within 50-feet of right-of-way if desired. If street trees are desired, an 8-foot wide planter strip is required. For detached sidewalk located outside of the right-of-way, the applicant may reduce the right-of-way width to 2-feet behind the back of sidewalk and provide a permanent right-of-way easement that extends from the right-of-way line to 2-feet behind the back of sidewalk.

In order to construct the temporary cul-de-sac at the terminus of Colona Way that is proposed off-site on the parcel directly north of the site, the applicant should be required to obtain approval and provide a temporary easement for the cul-de-sac from that property owner prior to ACHD's approval of the construction plans for the phase of the development that will construct Colona Way to stub to the site's north property line. If the applicant cannot obtain approval and a temporary cul-de-sac easement from the parcel owner to construct the cul-de-sac prior to ACHD's approval of the construction plans for the phase of the development that includes Colona Way, then the applicant should be required to construct a temporary cul-de-sac within the site at the terminus of Colona Way at the site's north property line. The cul-de-sac shall be

constructed per District Policy's standard cul-de-sac design requirements for local roads. See Finding 6.

## 5. Roadway Offsets

a. **Existing Conditions:** There are no existing roadways within the site.

b. **Policy:**

**Collector Offset Policy:** District policy 7206.4.2 states that the preferred spacing for new collectors intersecting existing collectors is ¼ mile to allow for adequate signal spacing and alignment.

District policy 7206.4.5, states that the preferred spacing for a new local street intersecting a collector roadway to align or offset a minimum of 330-feet from any other street (measured centerline to centerline).

**Local Offset Policy:** District policy 7207.4.2, requires local roadways to align or provide a minimum offset of 125-feet from any other street (measured centerline to centerline).

c. **Applicant's Proposal:** The applicant has proposed to construct one collector road, Wilder Farms Drive, to intersect Palmer Lane 1,650-feet north of SH-44.

The applicant has proposed to construct Tallis Avenue, a collector road, to intersect Wilder Farms Drive, a collector road, on the south side of the road 530-feet west of Palmer Lane.

The applicant has proposed to construct a local road, Hinsdale Drive, to intersect Hamlin Avenue, a collector road, 230-feet north of Schultz Street which is located off-site.

The applicant has proposed to construct Simsbury Street, a local road, to intersect Tallis Avenue, a collector road, on the east and west sides of the street 195-feet south of Wilder Farms Drive,

The applicant has proposed to construct 3 local roads to intersect a collector road, Wilder Farms Drive:

- Tallis Avenue on the north side of Wilder farms Drive 530-feet west of Palmer Lane,
- Regent Street on the east and west sides of Wilder Farms Drive located 145-feet south of the site's north property line, and
- Shelton Drive on the south side of Wilder Farms Drive 355-feet south of Regent Street.

d. **Staff Comments/Recommendations:** The applicant's proposal to construct Wilder Farms Drive to intersect Palmer Lane, Tallis Avenue to intersect Wilder Farms Drive and the local roads to intersect Wilder Farms Drive meets District Policy and should be approved, as proposed.

The applicant's proposal to construct Simsbury Street to intersect Tallis Avenue, a collector road, 195-feet south of Wilder Farms Drive does not meet District Policy which requires local roads intersecting a collector to be offset a minimum of 330-feet from any other street. However, staff recommends a modification of Policy to allow the applicant's proposal due to the fact that the length of Tallis Avenue, 307-feet, restricts the applicant from meeting District Policy. The City of Star has requested that the alignment of Wilder Farms Drive be circuitous through the site and intersect Palmer Lane which limits the applicants alignment options for a collector road that also extends to the site's south property line. This is a 41% modification of Policy and can be approved at the Development Services Manager Level since it is a dimension modification on a collector road.

The applicant's proposal to construct Hinsdale Drive to intersect Hamlin Avenue, a collector road, located off-site does not meet District Policy which requires local roads intersecting a collector road to be offset a minimum of 330-feet from any other street. However, staff

recommends a modification of Policy to allow the applicant's proposal due to the fact that this roadway provides access across the irrigation ditch for circulation and connectivity to Hamlin Avenue for the site. This is a 31% modification of Policy and can be approved at the Development Services Manager Level since it is a dimension modification on a collector road.

## 6. Stub Streets

a. **Existing Conditions:** There are no roads that stub to the site.

b. **Policy:**

**Stub Street Policy:** District policy 7206.2.4.3 (collector)/ 7207.2.4.3 (local) states that stub streets will be required to provide circulation or to provide access to adjoining properties. Stub streets will conform with the requirements described in Section 7206.2.4 (collector)/ 7207.2.4 (local), except a temporary cul-de-sac will not be required if the stub street has a length no greater than 150-feet. A sign shall be installed at the terminus of the stub street stating that, "THIS ROAD WILL BE EXTENDED IN THE FUTURE." or "THIS IS A DESIGNATED COLLECTOR ROADWAY. THIS STREET WILL BE EXTENDED AND WIDENED IN THE FUTURE."

In addition, stub streets must meet the following conditions:

- A stub street shall be designed to slope towards the nearest street intersection within the proposed development and drain surface water towards that intersection; unless an alternative storm drain system is approved by the District.
- The District may require appropriate covenants guaranteeing that the stub street will remain free of obstructions.

**Temporary Dead End Streets Policy:** 7206.2.4.4 (collector)/ 7207.2.4.4 (local) requires that the design and construction for cul-de-sac streets shall apply to temporary dead end streets. The temporary cul-de-sac shall be paved and shall be the dimensional requirements of a standard cul-de-sac. The developer shall grant a temporary turnaround easement to the District for those portions of the cul-de-sac which extend beyond the dedicated street right-of-way. In the instance where a temporary easement extends onto a buildable lot, the entire lot shall be encumbered by the easement and identified on the plat as a non-buildable lot until the street is extended.

c. **Applicant Proposal:** The applicant has proposed the following:

- Construct Wilder Farms Drive, a collector road, to stub to the site's north property line and locate it 1,145-feet to the west of the site's east property line.
- Construct Tallis Avenue, a collector road, to stub to the site's south property and locate it 488-feet west of the site's east property line.
- Construct Colona Way, a local road, to stub to the site's north property line and locate it 315-feet east of the site's west property line. A temporary turnaround is proposed to be constructed at the terminus of this road off-site.
- Construct Hinsdale Drive, a local road, to the site's west property line and locate it 253-feet north of the site's south property line. This road is proposed to be extended off-site to intersect Hamlin Avenue.
- Construct Bluford Street, a local road, to stub to the site's west property line and locate it 340-feet south of the site's north property line.

d. **Staff Comments/Recommendations:** The applicant's proposal meets District Policy and should be approved, as proposed. The applicant should be required to install signs at the terminus Bluford Street, stating that, "THIS ROAD WILL BE EXTENDED IN THE FUTURE".

Bluford Street, Tallis Avenue and Wilder Farms Drive are proposed to be less than 150-feet in length, therefore, a temporary cul-de-sac is not required at the terminus of those streets.

Per Findings 3 and 4, the applicant should be required to obtain approval from the property owner directly west of the site, and provide the legal description and exhibit for the right-of-way dedication to ACHD in order to construct Hamlin Avenue to intersect Hinsdale Drive off-site prior to ACHD's approval of the construction plans that include the construction of Hinsdale Drive to the site's west property line. If the applicant is not able to obtain approval from this property owner, then the applicant should be required to stub Hinsdale Drive to the site's west property line located 253-feet of the site's south property line, as proposed, and install a sign at the terminus stating that "THIS ROAD WILL BE EXTENDED IN THE FUTURE". This stub street would be less than 150-feet long, therefore, a temporary cul-de-sac is not required at the terminus of the street.

The applicant's proposal to construct Colona Way to stub to the site's north property line and construct an off-site temporary cul-de-sac meets District Policy and should be approved, as proposed. The applicant should be required to install a sign at the terminus of Colona Way stating that "THIS ROAD WILL BE EXTENDED IN THE FUTURE". Per Finding 4, in order to construct the temporary cul-de-sac off-site on the parcel directly north of the site at the terminus of Colona Way, the applicant should be required to obtain approval and provide a temporary easement for the cul-de-sac from that property owner prior to ACHD's approval of the construction plans for the phase of the development that will construct Colona Way to stub to the site's north property line. If the applicant cannot obtain approval and a temporary cul-de-sac easement from the parcel owner to construct the cul-de-sac prior to ACHD's approval of the construction plans for that phase of the development, then the applicant should be required to construct a temporary cul-de-sac within the site at the terminus of Colona Way at the site's north property line.

Temporary cul-de-sacs shall be paved and shall be the dimensional requirements of a standard cul-de-sac. The developer shall grant a temporary turnaround easement to the District for those portions of the cul-de-sac which extend beyond the dedicated street right-of-way. In the instance where a temporary easement extends onto a buildable lot, the entire lot shall be encumbered by the easement and identified on the plat as a non-buildable lot until the street is extended.

## **7. Driveways**

### **7.1 Palmer Lane and Wilder Farms Drive**

- a. Existing Conditions:** There is one existing 20-foot wide gravel driveway from the site onto Palmer Lane located 165-feet north of the site's south property line.

- b. Policy:**

**Access Policy:** District Policy 7205.4.1 states that all access points associated with development applications shall be determined in accordance with the policies in this section and Section 7202. Access points shall be reviewed only for a development application that is being considered by the lead land use agency. Approved access points may be relocated and/or restricted in the future if the land use intensifies, changes, or the property redevelops.

District Policy 7206.1 states that the primary function of a collector is to intercept traffic from the local street system and carry that traffic to the nearest arterial. A secondary function is to service adjacent property. Access will be limited or controlled. Collectors may also be designated at bicycle and bus routes.

**Driveway Location Policy (Stop Controlled Intersection):** District policy 7206.4.4 requires driveways located on collector roadways near a STOP controlled intersection to be located outside of the area of influence; OR a minimum of 150-feet from the intersection, whichever is



greater. Dimensions shall be measured from the centerline of the intersection to the centerline of the driveway.

**Successive Driveways:** District policy 7206.4.5 Table 1, requires driveways located on collector roadways with a speed limit of 45 MPH and daily traffic volumes greater than 100 VTD to align or offset a minimum of 285-feet from any existing or proposed driveway.

**Driveway Width Policy:** District policy 7206.4.6 restricts high-volume driveways (100 VTD or more) to a maximum width of 36-feet and low-volume driveways (less than 100 VTD) to a maximum width of 30-feet. Curb return type driveways with 30-foot radii will be required for high-volume driveways with 100 VTD or more. Curb return type driveways with 15-foot radii will be required for low-volume driveways with less than 100 VTD.

**Driveway Paving Policy:** Graveled driveways abutting public streets create maintenance problems due to gravel being tracked onto the roadway. In accordance with District policy, 7206.4.6, the applicant should be required to pave the driveway its full width and at least 30-feet into the site beyond the edge of pavement of the roadway and install pavement tapers in accordance with Table 2 under District Policy 7206.4.6.

- c. **Applicant's Proposal:** The applicant has proposed to close the existing driveway from the site onto Palmer Lane with curb, gutter and sidewalk.

The applicant has proposed to construct one curb cut type driveway to access the pool facilities on the south side of Wilder Farms Drive located 160-feet north of Shelton Drive, a local road.

- d. **Staff Comments/Recommendations:** The applicant's proposal meets District Policy and should be approved, as proposed. The applicant should be required to construct the driveway on Wilder Farms Drive as a curb return type driveway.

## 6.2 Simsbury Street and Gramville Avenue

- a. **Existing Conditions:** There are no local roads within the site.

b. **Policy:**

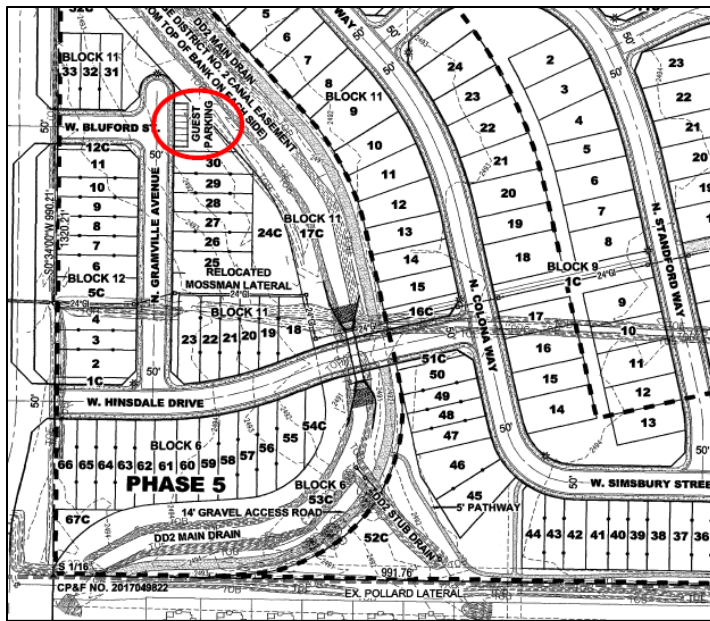
**Driveway Location Policy:** District policy 7207.4.1 requires driveways near intersections to be located a minimum of 75-feet (measured centerline-to-centerline) from the nearest local street intersection, and 150-feet from the nearest collector or arterial street intersection.

**Successive Driveways:** District Policy 7207.4.1 states that successive driveways away from an intersection shall have no minimum spacing requirements for access points along a local street, but the District does encourage shared access points where appropriate.

**Driveway Width Policy:** District policy 7207.4.3 states that where vertical curbs are required, residential driveways shall be restricted to a maximum width of 20-feet and may be constructed as curb-cut type driveways.

**Driveway Paving Policy:** Graveled driveways abutting public streets create maintenance problems due to gravel being tracked onto the roadway. In accordance with District policy, 7207.4.3, the applicant should be required to pave the driveway its full width and at least 30-feet into the site beyond the edge of pavement of the roadway.

- c. **Applicant's Proposal:** The applicant has proposed to construct 7 perpendicular parking stalls located outside of the right-of-way that will back onto the local road Gramville Avenue, shown below.



- d. **Staff Comments/Recommendations:** The applicant's proposal to construct perpendicular parking that will back directly onto Granville Avenue is not approved as proposed. Staff has concerns about ADA compliance and cars backing into traffic, especially near an intersection. If additional parking is necessary to serve the site, then an off-site parking lot should be constructed outside of the right-of-way.

## 8. Traffic Calming

- a. **Speed Control and Traffic Calming Policy (Local):** District policy 7207.3.7 states that the design of local street systems should discourage excessive speeds by using passive design elements. If the design or layout of a development is anticipated to necessitate future traffic calming implementation by the District, or the streets extend greater than 750-feet in length, then the District will require changes to the layout and/or the addition of passive design elements such as horizontal curves, bulb-outs, chokers, etc. The District will also consider texture changes to the roadway surface (i.e. stamped concrete) as a passive design element. These alternative methods may require maintenance and/or license agreement.
- b. **Staff Comments/Recommendations:** Simsbury Street and Colona Way are local roads that are proposed to have straight segments of roadway that are longer than 750-feet within the development. The applicant should be required to redesign the roadways or provide passive traffic calming measures consistent with District Policy with the revised preliminary plat. Stop signs, speed humps and valley gutters will not be accepted as traffic calming.

## 9. Bridge for Lateral 12 Crossing and Mossman Lateral Pipe Crossings, and Irrigation Lateral Crossing for Hamlin Avenue (off-site)

The District will require that the applicant submit the bridge plans for the crossing of Lateral 12 (Hinsdale Drive), the Mossman Lateral pipe crossings throughout the site and the off-site lateral crossing for Hamlin Avenue for review and approval prior to the pre-construction meeting and final plat approval. Note: all plan submittals for bridges or pipe crossings of irrigation facilities should be submitted to ACHD for review no later than December 15<sup>th</sup> for construction in the following year prior to irrigation season.

## 10. Tree Planters

**Tree Planter Policy:** Tree Planter Policy: The District's Tree Planter Policy prohibits all trees in planters less than 8-feet in width without the installation of root barriers. Class II trees may be

allowed in planters with a minimum width of 8-feet, and Class I and Class III trees may be allowed in planters with a minimum width of 10-feet.

## **11. Landscaping**

**Landscaping Policy:** A license agreement is required for all landscaping proposed within ACHD right-of-way or easement areas. Trees shall be located no closer than 10-feet from all public storm drain facilities. Landscaping should be designed to eliminate site obstructions in the vision triangle at intersections. District Policy 5104.3.1 requires a 40-foot vision triangle and a 3-foot height restriction on all landscaping located at an uncontrolled intersection and a 50-foot offset from stop signs. Landscape plans are required with the submittal of civil plans and must meet all District requirements prior to signature of the final plat and/or approval of the civil plans.

## **12. Other Access**

Palmer Lane, Tallis Avenue and Wilder Farm Drive are classified as collector roadways. Other than the access specifically approved with this application, direct lot access is prohibited to these roadways and should be noted on the final plat.

## **D. Site Specific Conditions of Approval**

1. Submit a revised preliminary plat to ACHD for review prior to submitting the construction plans for the development to ACHD that includes the following Simsbury Street and Colona Way redesigned to have less than 750-feet of straight long segments or provide passive traffic calming measures consistent with District Policy. Stop signs, speed humps and valley gutters will not be accepted as traffic calming.
2. The applicant's proposal to construct perpendicular parking that will back directly onto Gramville Avenue is not approved.
3. Widen the pavement on Palmer Lane to 30-feet from the site's south property line to SH-44 or provide 24-feet of pavement with 3-foot wide gravel shoulders and a minimum 6-foot wide detached asphalt/concrete pedestrian facility from the site's south property line to SH-44.
4. Dedicate additional right-of-way for Palmer Lane abutting the site to total 37-feet from the centerline of the road, as proposed.
5. Improve Palmer Lane as ½ of a 36-foot wide collector street section with curb, gutter and 5-foot wide detached of 7-foot wide attached concrete sidewalk, as proposed.
6. Construct one collector roadway, Wilder Farms Drive, to intersect Palmer Lane 1,650-feet north of SH-44, as proposed.
7. Construct one collector road, Tallis Avenue, to intersect Wilder Farms Drive 530-feet west of Palmer Lane, as proposed.
8. Construct all collector streets within the site as 36-foot wide collector street sections with vertical curb, gutter, 8-foot wide planter strips within 50-feet of right-of-way and 5-foot wide detached concrete sidewalks located outside of the right-of-way, as proposed.
9. Construct the entry portion of Wilder Farms Drive as a 52-foot wide street section with 21-foot wide travel lanes, a 12-foot wide landscape median, vertical curb, gutter, 8-foot wide planter strips within 60-feet of right-of-way and 5-foot wide detached concrete sidewalks located outside of the right-of-way.
10. Construct one collector road, Wilder Farms Drive, to stub to the site's north property line located 1,145-feet west of the site's east property line, as proposed.
11. Construct one collector road, Tallis Avenue, to stub to the site's south property line located 488-feet west of the site's east property line, as proposed.

12. Install a sign at the terminus of Wilder Farms Drive and Tallis Avenue that will stub to the site's north and south property lines stating that, "THIS IS A DESIGNATED COLLECTOR ROADWAY. THIS STREET WILL BE EXTENDED AND WIDENED IN THE FUTURE." A temporary cul-de-sac is required at the terminus of Wilder Farms Drive and Tallis Avenue.
13. Obtain approval from the property owner directly west of the site to construct the off-site roads Hamlin Avenue and Hinsdale Avenue, and provide the legal description and exhibit for the right-of-way dedication to ACHD prior to ACHD's approval of the construction plans that include the construction of Hinsdale Drive to the site's west property line.
14. Construct Hamlin Avenue, a collector road, off-site extending from Schultz Street to the north to Hinsdale Drive, as proposed, and construct it as a 30-foot wide with a minimum of 30-feet of pavement and 3-foot wide gravel shoulders, or provide 24-feet of pavement with 3-foot wide gravel shoulders and a minimum 6-foot wide detached asphalt/concrete pedestrian facility within 76-feet of right-of-way. The applicant may construct Hamlin Avenue off-site as 36-foot wide collector street section with curb, gutter and 5-foot wide detached or 7-foot wide attached concrete sidewalk within 76-feet of right-of-way if desired.
15. Extend Hinsdale Drive to intersect Hamlin Avenue 230-feet north of Schultz Street off-site, as proposed. Construct the off-site segment of Hinsdale Drive with a minimum of 24-feet of pavement and 3-foot wide gravel shoulders within a minimum of 50-feet of right-of-way. The applicant may construct Hinsdale Avenue off-site as a 36-foot wide local street section with curb, gutter and 5-foot wide concrete sidewalk within 50-feet of right-of-way if desired.
16. If the applicant is not able to obtain approval from the property owner to construct Hinsdale Drive and Hamlin Avenue off-site as proposed, then stub Hinsdale Drive to the site's west property line located 253-feet north of the site's south property line and install a sign at the terminus stating that "THIS ROAD WILL BE EXTENDED IN THE FUTURE". A temporary cul-de-sac is not required at this terminus since it would be less than 150-feet long.
17. Construct Simsbury Street, a local road, to intersect Tallis Avenue, a collector road, on the east and west sides of the street located 195-feet south of Wilder Farms Drive, as proposed.
18. Construct Tallis Avenue, a local road, to intersect Wilder Farms Drive on the north side of the street located 530-feet west of Palmer Lane, as proposed.
19. Construct Regent Street, a local road, to intersect Wilder Farms Drive on the east and west sides of the street located 145-feet south of the site's north property line, as proposed.
20. Construct Shelton Drive, a local road, to intersect Wilder Farms Drive on the south side of the street located 355-feet south of Regent Street, as proposed.
21. Construct a 25-foot wide curb return type driveway on Wilder Farms Drive located 160-feet north of Shelton Drive, as proposed.
22. Construct all internal local roads as 36-foot wide local street sections with curb, gutter and 5-foot wide concrete sidewalk within 50-feet of right-of-way, as proposed.
23. If street trees are desired, an 8-foot wide planter strip is required.
24. For detached sidewalk located outside of the right-of-way, the applicant may reduce the right-of-way width to 2-feet behind the back of sidewalk and provide a permanent right-of-way easement that extends from the right-of-way line to 2-feet behind the back of sidewalk.
25. Construct Bluford Street, a local road, to stub to the site's west property line and locate 340-feet south of the site's north property line, as proposed.
26. Construct Colona Way, a local road, to stub to the site's north property line and locate it 315-feet east of the site's west property line, as proposed.



27. Install a sign at the terminus of Colona Way and Bluford Street stating that "THIS ROAD WILL BE EXTENDED IN THE FUTURE".
28. Construct a temporary cul-de-sac at the terminus of Colona Way, as proposed. If the temporary cul-de-sac will be located off-site on the parcel directly north of the site, as proposed, obtain approval and provide a temporary easement for the cul-de-sac from that property owner prior to ACHD's approval of the construction plans for the phase of the development that will construct Colona Way to stub to the site's north property line.
29. Temporary cul-de-sacs at the terminus of stub streets shall be paved and shall be the dimensional requirements of a standard cul-de-sac. Grant a temporary turnaround easement to the District for those portions of the cul-de-sac which extend beyond the dedicated street right-of-way. In the instance where a temporary easement extends onto a buildable lot, the entire lot shall be encumbered by the easement and identified on the plat as a non-buildable lot until the street is extended.
30. Construct 5 knuckles, 3 with landscape islands, as proposed.
31. Construct 2 cul-de-sacs with landscape islands, as proposed.
32. Close the existing 20-foot wide gravel driveway from the site onto Palmer Lane located 165-feet north of the site's south property line with curb, gutter and sidewalk with the improvements for Palmer Lane abutting the site, as proposed.
33. Plat landscape medians and islands as right-of-way owned by ACHD. The applicant or homeowners association shall enter into a license agreement with ACHD for the maintenance of the landscape medians and islands.
34. Submit the bridge plans for the crossing of Lateral 12 (Hinsdale Drive), the Mossman Lateral pipe crossings throughout the site and the off-site lateral crossing for Hamlin Avenue for review and approval prior to the pre-construction meeting and final plat approval. Note: all plan submittals for bridges or pipe crossings of irrigation facilities should be submitted to ACHD for review no later than December 15<sup>th</sup> for construction in the following year prior to irrigation season.
35. Direct lot access is prohibited to Palmer Lane, Tallis Avenue and Wilder Farms Drive and should be noted on the final plat.
36. Submit civil plans to ACHD Development Services for review and approval. The impact fee assessment will not be released until the civil plans are approved by ACHD.
37. Payment of impact fees is due prior to issuance of a building permit.
38. Comply with all Standard Conditions of Approval.

## **E. Standard Conditions of Approval**

1. All proposed irrigation facilities shall be located outside of the ACHD right-of-way (including all easements). Any existing irrigation facilities shall be relocated outside of the ACHD right-of-way (including all easements).
2. Private Utilities including sewer or water systems are prohibited from being located within the ACHD right-of-way.
3. In accordance with District policy, 7203.3, the applicant may be required to update any existing non-compliant pedestrian improvements abutting the site to meet current Americans with Disabilities Act (ADA) requirements. The applicant's engineer should provide documentation of ADA compliance to District Development Review staff for review.

4. Replace any existing damaged curb, gutter and sidewalk and any that may be damaged during the construction of the proposed development. Contact Construction Services at 387-6280 (with file number) for details.
5. A license agreement and compliance with the District's Tree Planter policy is required for all landscaping proposed within ACHD right-of-way or easement areas.
6. All utility relocation costs associated with improving street frontages abutting the site shall be borne by the developer.
7. It is the responsibility of the applicant to verify all existing utilities within the right-of-way. The applicant at no cost to ACHD shall repair existing utilities damaged by the applicant. The applicant shall be required to call DIGLINE (1-811-342-1585) at least two full business days prior to breaking ground within ACHD right-of-way. The applicant shall contact ACHD Traffic Operations 387-6190 in the event any ACHD conduits (spare or filled) are compromised during any phase of construction.
8. Utility street cuts in pavement less than five years old are not allowed unless approved in writing by the District. Contact the District's Utility Coordinator at 387-6258 (with file numbers) for details.
9. All design and construction shall be in accordance with the ACHD Policy Manual, ISPWC Standards and approved supplements, Construction Services procedures and all applicable ACHD Standards unless specifically waived herein. An engineer registered in the State of Idaho shall prepare and certify all improvement plans.
10. Construction, use and property development shall be in conformance with all applicable requirements of ACHD prior to District approval for occupancy.
11. No change in the terms and conditions of this approval shall be valid unless they are in writing and signed by the applicant or the applicant's authorized representative and an authorized representative of ACHD. The burden shall be upon the applicant to obtain written confirmation of any change from ACHD.
12. If the site plan or use should change in the future, ACHD Planning Review will review the site plan and may require additional improvements to the transportation system at that time. Any change in the planned use of the property, which is the subject of this application, shall require the applicant to comply with ACHD Policy and Standard Conditions of Approval in place at that time unless a waiver/variance of the requirements or other legal relief is granted by the ACHD Commission.

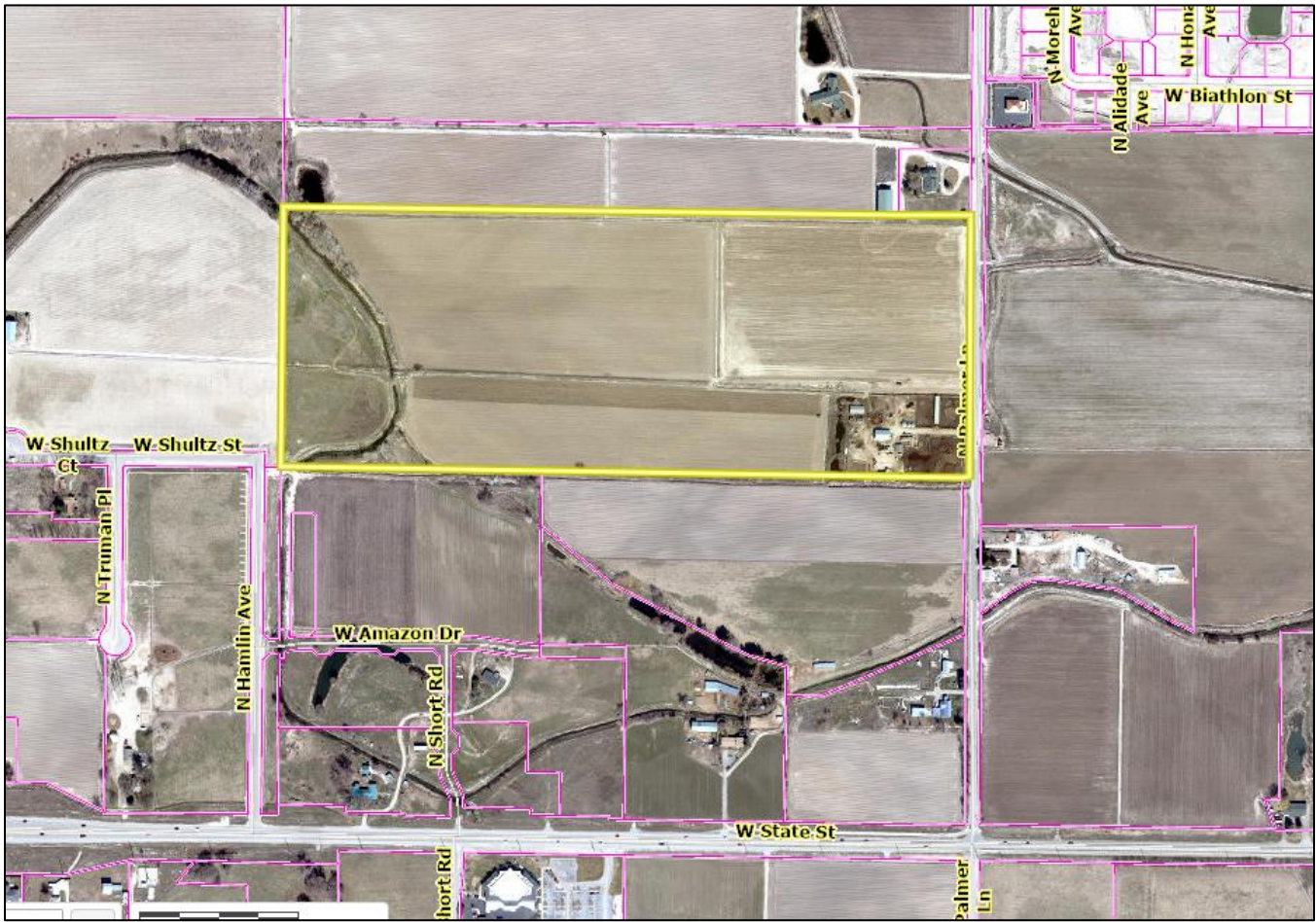
## **F. Conclusions of Law**

1. The proposed site plan is approved, if all of the Site Specific and Standard Conditions of Approval are satisfied.
2. ACHD requirements are intended to assure that the proposed use/development will not place an undue burden on the existing vehicular transportation system within the vicinity impacted by the proposed development.

## **G. Attachments**

1. Vicinity Map
2. Site Plan
3. TIS Executive Summary
4. Utility Coordinating Council
5. Development Process Checklist
6. Request for Reconsideration Guidelines OR Appeal Guidelines

VICINITY MAP







## EXECUTIVE SUMMARY

Challenger Development, Inc. retained CR Engineering, Inc. to prepare a traffic impact study (TIS) for the proposed Fountain Park Subdivision located north of SH 44 between Hamlin Avenue and Palmer Lane in Star, Idaho, as shown in Figure 1.1. The scope of the TIS was determined through coordination with the Idaho Transportation Department (ITD) and Ada County Highway District (ACHD) with inputs from the Community Planning Association of Southwest Idaho (COMPASS).

The TIS evaluates the potential traffic impacts resulting from background traffic growth, in-process developments in the area, and the proposed development, and identifies improvements to mitigate the impacts if needed. Traffic impacts were evaluated under weekday AM and PM peak hour traffic conditions with the proposed land uses and site access as shown in the preliminary site plan. Table 1 summarizes the improvements needed to mitigate the traffic impacts for the following analysis years traffic conditions:

- 2020 Existing traffic
- 2025 Build-out year background traffic
- 2025 Build-out year total traffic

**Table 1 – Study Area Intersection and Roadway Improvements Summary**

| Intersection<br>or<br>Roadway Segment                        | 2020<br>Existing      | 2025 Build-Out Year  |  |
|--|-----------------------|--|--|
|  |                       | Background   | Total  |
| ① S Floating Feather Road and Palmer Lane                    | None                  | None   | None   |
| ② SH 16 and SH 44  | Adjust signal timings | SH 44 widened to five lanes east and west of intersection <sup>1,3</sup>                           | SH 44 widened to five lanes east and west of intersection <sup>1,3</sup> |
| ③ Hamlin Avenue and SH 44                                    | None                  | SH 44 widened to five lanes, <sup>1</sup> traffic separator median to be installed by Amazon Falls | None beyond prior improvements   |
| ④ Palmer Lane and SH 44                                      | None                  | SH 44 widened to five lanes <sup>1, 2</sup> and traffic signal                                     | SH 44 widened to five lanes <sup>1, 2</sup> and traffic signal           |
| ⑤ Wilder Farms Street and Palmer Lane (proposed site access) | N/A                   | N/A  | Stop-controlled T intersection   |
| Palmer Lane, S Floating Feather Road to Wilder Farms Street  | None                  | None   | None <sup>4</sup>  |
| Palmer Lane, Wilder Farms Street to SH 44                    | None                  | None   | None <sup>4</sup>  |
| Hamlin Ave   | None                  | None   | None   |

<sup>1</sup> SH 44 is funded to be widened to five lanes in 2023 east of SH 16 and in 2024 west of SH 16

<sup>2</sup> Intersection exceeds ITD minimum operational thresholds with ITIP widening as an unsignalized intersection

<sup>3</sup> One or more lane groups operate with a volume to capacity ratio (v/c) exceeding ITD 0.90 threshold but less than ACHD 1.00 threshold

<sup>4</sup> Improve half of roadway along the site frontages to ACHD residential collector street standards

## 1.0 Proposed Development

- 1.1 Fountain Park Subdivision is estimated to contain 173 single-family dwelling units and 78 townhomes. The expected build-out year is 2025 but may change depending on the market conditions.
- 1.2 Based on the procedures outlined in the *Trip Generation Manual, 10<sup>th</sup> Edition*, published by the Institute of Transportation Engineers (ITE), Fountain Park Subdivision is estimated to generate approximately 2,271 trips per weekday, 166 trips during the AM peak hour, and 219 trips during the PM peak hour.
  - The development is not expected to retain internal capture trips or generate pass-by trips
  - All trips generated by the development were assumed to be made by personal or commercial vehicles
- 1.3 The estimated site traffic distribution patterns are:
  - 10% west of the site along SH 44
  - 35% south of the site along SH 16
  - 40% east of the site along SH 44
  - 10% north of the site along Palmer Road
  - 5% east of the site along S Floating Feather Road
- 1.4 Fountain Park Subdivision is planning to construct Wilder Farms Street, a collector street, connecting to Palmer Lane for site access:
  - Wilder Farms Street is located approximately 1,760 feet south of Nordic Drive and 1,650 feet north of SH 44
    - Meets minimum collector street intersection spacing guidelines of ¼-mile spacing on Palmer Lane, a collector street
  - The proposed site access intersection of Wilder Farms Street and Palmer Lane is anticipated to meet ACHD minimum operational thresholds
  - No turn lanes are warranted under 2025 build-out year total traffic conditions based on ACHD turn lane guidelines
  - Estimated to have adequate intersection sight distance based on field review
- 1.5 Fountain Park Subdivision is planning to construct Hinsdale Drive, a local street, connecting to Hamlin Avenue to provide a second site access to the transportation system:
  - The owner of the property west of the site has an agreement with the developer to provide right-of-way for constructing off-site streets connecting to Hamlin Avenue
  - Hinsdale Drive is located approximately 210 feet north of Shultz Street
    - Meets minimum local road spacing of 150 feet on Hamlin Avenue, a local road
- 1.6 All internal roadways are anticipated to carry less than 1,000 vehicles per day except for one roadway:
  - Wilder Farms Street
    - Anticipated to carry approximately 1,960 vehicles per day at full build-out of Fountain Park Subdivision
    - Should be classified as a collector street and designed to ACHD collector street standards
    - Meets the ACHD level of service planning threshold of 425 vehicles per hour (vph) as a two-lane collector street
      - The anticipated peak hour directional volume is 129 vph in the PM peak hour

## 2.0 2020 Existing Traffic Conditions

- 2.1 Due to road closures and construction on Palmer Lane, no new traffic counts were collected for this TIS. Old traffic counts collected between October 2018 and December 2019 for previous studies in the area were used in this TIS. The 2018/2019 traffic counts were scaled up to 2020 existing traffic by using a 4.0% annual growth rate on all study area roadways based on COMPASS model forecasts and consistent with prior studies in the area.
- 2.2 With the estimated 2020 existing traffic, all study area roadway segments meet ACHD level of service planning thresholds with the existing lane configuration. As a result, no roadway capacity improvements are needed to mitigate 2020 existing traffic operations.
- 2.3 With 2020 existing traffic, two study area intersections exceed ITD and/or ACHD minimum operational thresholds analyzed with the existing intersection control and lane configurations. None of the other study area intersections require turn lanes based on ITD or ACHD turn-lane guidelines. The intersections, operational deficiencies, and proposed mitigation improvements are:
- SH 16 and SH 44 intersection
    - The intersection meets minimum operational thresholds, except for one lane group. With existing ACHD signal timing, the eastbound through lane group operates with a volume to capacity ratio (v/c) of 1.08 during the AM peak hour, exceeding ITD 0.90 threshold and ACHD 1.00 threshold
    - The following improvements are proposed to mitigate 2020 existing traffic operations:
      - Signal timing adjustments
        - Adjusting the signal timing to allot more green time for the eastbound through movement reduces the v/c ratio to 0.92, while the intersection and all other lane groups continue to meet minimum operational thresholds
  - Palmer Lane and SH 44 intersection
    - The critical southbound lane group operates at Level of Service (LOS) E during the PM peak hour, exceeding ITD LOS D threshold but meets ACHD LOS E threshold. Additionally, the southbound lane group operates with a v/c ratio of 0.44, meeting ITD and ACHD threshold
    - No improvements are proposed to mitigate 2020 existing traffic operations
      - The intersection is planned to be signalized by 2027 but does not meet MUTCD signal warrants with 2020 existing traffic

## 3.0 2025 Build-Out Year Background Traffic Conditions (No Site Traffic)

- 3.1. 2025 background traffic was estimated by extrapolating the existing traffic counts with a 4.0% annual growth rate based. In addition, in-process traffic from Amazon Falls Subdivision, Amazon Falls Subdivision No. 2, and Moon Valley Subdivision were included in 2025 background traffic to more accurately reflect traffic volumes on Palmer Lane.
- 3.2. The following roadway improvements are expected to be constructed by 2025 and were included in the 2025 background traffic analysis:
- Widen SH 44 to five lanes from Highbrook Way to Linder Road
    - SH 44 widening from SH 16 to Linder Road is programmed for construction in 2023 according to ITD FY2021-2027 *Idaho Transportation Investment Program* (ITIP) (Project No. 20266)
    - SH 44 widening from west of Highbrook Way to SH 16 is programmed for construction in 2024 according to ITD FY2021-2027 ITIP (Project No. 20574)

- 3.3. The Palmer Lane and SH 44 intersection is programmed in the ITIP to be signalized by 2027
- ITD and ACHD have set up a road trust fund and are collecting proportionate share contributions from in-process developments to fund the signal construction
- 3.4. The following intersection improvements are conditioned by ITD to mitigate the impacts generated by the proposed Amazon Falls Subdivision, which is currently under construction:
- Hamlin Avenue and SH 44 intersection
    - Construct traffic separator median on SH 44 to limit Hamlin Avenue to right-in and right-out (RIRO) movements only
- 3.5. With 2025 background traffic, three study area intersections are expected to exceed ITD and/or ACHD minimum operational thresholds analyzed with the existing intersection control and lane configurations (no-build) or with ITIP improvements on SH 44 and in-process development mitigation improvements identified above. The intersections, operational deficiencies, and proposed mitigation improvements are:
- SH 16 and SH 44 intersection
    - Without ITIP widening on SH 44, the intersection is anticipated to exceed ITD and ACHD minimum operational thresholds
      - The overall intersection operates at LOS F with a v/c ratio of 0.97 during the AM peak hour and 1.00 during the PM peak hour
      - Several lane groups are anticipated to operate with v/c ratios exceeding 1.00
    - With ITIP widening on SH 44, the intersection is anticipated to meet ITD and ACHD minimum operational thresholds with the exception of two lane groups operating with v/c ratios exceeding ITD 0.90 threshold
      - The eastbound through lane group operates with a v/c ratio of 0.93 during the AM peak hour
      - The westbound through lane group operates with a v/c ratio of 0.92 during the PM peak hour
    - No additional improvements beyond ITIP program improvements on SH 44 are proposed to mitigate 2025 background traffic operations
      - The SH 16 and SH 44 intersection is planned to be reconstructed as a grade-separated interchange in the long-term in 2045
      - The interchange construction year or funding is not available at the time of this TIS
    - The intersection fails to meet ITD and/or ACHD minimum operational thresholds under 2020 existing traffic conditions; therefore, it will continue to fail with additional background traffic growth if the programmed ITIP improvements are not constructed
  - Hamlin Avenue and SH 44 intersection
    - Without ITIP widening on SH 44, the critical southbound lane group is anticipated to operate at LOS E with a v/c ratio of 0.01 during the PM peak hour as a RIRO intersection, exceeding ITD LOS D threshold but meet ACHD minimum operational thresholds
    - With ITIP widening on SH 44, the critical southbound right-turn lane group is anticipated to operate at LOS C or better with a v/c ratio less than 0.01 during the peak hours as a RIRO intersection, meeting ITD and ACHD minimum operational thresholds
    - No additional improvements beyond the RIRO restrictions are proposed to mitigate 2025 background traffic operations



- The westbound left-turn lane group is anticipated to operate with a v/c ratio of 0.91 during the AM peak hour and 0.95 during the PM peak hour
- The northbound through lane group is anticipated to operate with a v/c ratio of 1.00 during the PM peak hour
- No additional improvements beyond ITIP widening on SH 44 are proposed to mitigate 2025 total traffic operations
  - The SH 16 and SH 44 intersection is planned to be reconstructed as a grade-separated interchange in the long-term in 2045
  - The interchange construction year or funding is not available at the time of this TIS
- ITD will assess a proportionate share of the development impacts to fund the interchange construction
- The intersection fails to meet ITD and/or ACHD minimum operational thresholds under 2020 existing traffic conditions; therefore, it will continue to fail with additional background traffic growth and site traffic if the programmed ITIP improvements are not constructed

#### ■ Hamlin Avenue and SH 44 intersection

- Without ITIP widening on SH 44, the critical southbound lane group is anticipated to operate at LOS E with a v/c ratio of 0.26 during the PM peak hour, exceeding ITD LOS D threshold but meets ACHD minimum operational thresholds
- With ITIP widening on SH 44, the critical southbound right-turn lane group is anticipated to operate at LOS C or better with a v/c ratio of 0.09 during the PM peak hour as a RIRO intersection, meeting ITD and ACHD minimum operational thresholds
- As with 2025 background traffic, no additional improvements beyond the RIRO restrictions are proposed to mitigate 2025 total traffic operations
  - Without the ITIP improvements, the intersection is anticipated to exceed the ITD LOS D thresholds by 2022 with an additional traffic increase of approximately 310 vph in the AM peak hour and 365 vph in the PM peak hour beyond 2020 existing volumes
    - This additional traffic increase is equivalent to two year of background traffic growth plus site traffic generated by 90 single-family dwelling units in Phases 1 and 2 of Fountain Park Subdivision

#### ■ Palmer Lane and SH 44 intersection

- Without ITIP widening on SH 44, the northbound and southbound lane groups are anticipated to operate at LOS F with v/c ratios exceeding 1.00 during the PM peak hour as an unsignalized intersection, exceeding ITD and ACHD minimum operational thresholds
- With ITIP widening on SH 44, the critical southbound approach is anticipated to operate at LOS F with a v/c ratio of 0.62 during the AM peak hour and exceeding 1.00 during the PM peak hour as an unsignalized intersection, exceeding ITD and ACHD minimum operational thresholds
- As with 2025 background traffic, the following improvements are proposed to mitigate 2025 total traffic operations:
  - Install a traffic signal with the following lanes:
    - One left-turn lane, one through lane, and one shared through/right-turn lane on the eastbound and westbound approaches



- One left-turn lane and one shared through/right-turn lane on the northbound and southbound approaches
  - The intersection is anticipated to satisfy MUTCD Signal Warrants 1 & 2, eight-hour and four-hour vehicular volume, under 2025 total traffic conditions
    - The intersection is anticipated to meet MUTCD signal warrants by 2022 with an additional traffic increase of approximately 250 vph in the AM peak hour and 300 vph in the PM peak hour beyond 2020 existing volumes with and without the ITIP improvements
      - By 2022, Fountain Park Subdivision is assumed to have constructed Phases 1 and 2, containing 90 single-family dwelling units
      - ITD and ACHD should monitor the intersection, collect traffic counts, and conduct operational and signal warrant analysis to verify the intersection operations and timing of improvements needs
- 4.2 The estimated site traffic as a percentage of the 2025 total traffic at the study area intersections are:
- S Floating Feather Road and Palmer Lane intersection – AM peak = 6.1%, PM peak = 6.7%
  - SH 16 and SH 44 intersection – AM Peak = 1.9%, PM peak = 2.1%
  - Hamlin Avenue and SH 44 intersection – AM peak = 2.9%, PM peak = 3.5%
  - Palmer Lane and SH 44 intersection – AM peak = 4.0%, PM peak = 5.4%
- 4.3 With 2025 build-out year total traffic, all study area roadway segments are expected to continue to meet minimum operational thresholds with the existing lane configuration. As a result, no roadway capacity improvements are needed to mitigate 2025 total traffic operations beyond the intersection improvements at the Palmer Lane and SH 44 intersection. The estimated site traffic as a percentage of the 2025 total traffic on the study area roadway segments are:
- Palmer Lane – S Floating Feather Road to Wilder Farms Street – AM peak = 15.5%, PM peak = 15.8%
  - Palmer Lane – Wilder Farms Street to SH 44 – AM peak = 23.0%, PM peak = 34.2%
  - Hamlin Avenue – AM peak = 97.1%, PM peak = 95.7%

# Ada County Utility Coordinating Council

## Developer/Local Improvement District Right of Way Improvements Guideline Request

*Purpose: To develop the necessary avenue for proper notification to utilities of local highway and road improvements, to help the utilities in budgeting and to clarify the already existing process.*

- 1) **Notification:** Within five (5) working days upon notification of required right of way improvements by Highway entities, developers shall provide written notification to the affected utility owners and the Ada County Utility Coordinating Council (UCC). Notification shall include but not be limited to, project limits, scope of roadway improvements/project, anticipated construction dates, and any portions critical to the right of way improvements and coordination of utilities.
- 2) **Plan Review:** The developer shall provide the highway entities and all utility owners with preliminary project plans and schedule a plan review conference. Depending on the scale of utility improvements, a plan review conference may not be necessary, as determined by the utility owners. Conference notification shall also be sent to the UCC. During the review meeting the developer shall notify utilities of the status of right of way/easement acquisition necessary for their project. At the plan review conference each company shall have the right to appeal, adjust and/or negotiate with the developer on its own behalf. Each utility shall provide the developer with a letter of review indicating the costs and time required for relocation of its facilities. Said letter of review is to be provided within thirty calendar days after the date of the plan review conference.
- 3) **Revisions:** The developer is responsible to provide utilities with any revisions to preliminary plans. Utilities may request an updated plan review meeting if revisions are made in the preliminary plans which affect the utility relocation requirements. Utilities shall have thirty days after receiving the revisions to review and comment thereon.
- 4) **Final Notification:** The developer will provide highway entities, utility owners and the UCC with final notification of its intent to proceed with right of way improvements and include the anticipated date work will commence. This notification shall indicate that the work to be performed shall be pursuant to final approved plans by the highway entity. The developer shall schedule a preconstruction meeting prior to right of way improvements. Utility relocation activity shall be completed within the times established during the preconstruction meeting, unless otherwise agreed upon.

**Notification to the Ada County UCC can be sent to:** 50 S. Cole Rd. Boise 83707, or Visit [iducc.com](http://iducc.com) for e-mail notification information.

# Development Process Checklist

## Items Completed to Date:

- ☒ Submit a development application to a City or to Ada County
- ☒ The City or the County will transmit the development application to ACHD
- ☒ The ACHD **Planning Review Section** will receive the development application to review
- ☒ The **Planning Review Section** will do one of the following:
  - ☐ Send a **"No Review"** letter to the applicant stating that there are no site specific conditions of approval at this time.
  - ☒ Write a **Staff Level** report analyzing the impacts of the development on the transportation system and evaluating the proposal for its conformance to District Policy.
  - ☐ Write a **Commission Level** report analyzing the impacts of the development on the transportation system and evaluating the proposal for its conformance to District Policy.

## Items to be completed by Applicant:

- ☐ For **ALL** development applications, including those receiving a **"No Review"** letter:
  - The applicant should submit one set of engineered plans directly to ACHD for review by the **Development Review Section** for plan review and assessment of impact fees. (Note: if there are no site improvements required by ACHD, then architectural plans may be submitted for purposes of impact fee assessment.)
  - The applicant is required to get a permit from Construction Services (ACHD) for ANY work in the right-of-way, including, but not limited to, driveway approaches, street improvements and utility cuts.
- ☐ Pay Impact Fees prior to issuance of building permit. Impact fees cannot be paid prior to plan review approval.

### DID YOU REMEMBER:

#### *Construction (Non-Subdivisions)*

- ☐ **Driveway or Property Approach(s)**
  - Submit a "Driveway Approach Request" form to ACHD Construction (for approval by Development Services & Traffic Services). There is a one week turnaround for this approval.
- ☐ **Working in the ACHD Right-of-Way**
  - Four business days prior to starting work have a bonded contractor submit a "Temporary Highway Use Permit Application" to ACHD Construction – Permits along with:
    - a) Traffic Control Plan
    - b) An Erosion & Sediment Control Narrative & Plat, done by a Certified Plan Designer, if trench is >50' or you are placing >600 sf of concrete or asphalt.

#### *Construction (Subdivisions)*

- ☐ **Sediment & Erosion Submittal**
  - At least one week prior to setting up a Pre-Construction Meeting an Erosion & Sediment Control Narrative & Plan, done by a Certified Plan Designer, must be turned into ACHD Construction to be reviewed and approved by the ACHD Stormwater Section.
- ☐ **Idaho Power Company**
  - Vic Steelman at Idaho Power must have his IPCO approved set of subdivision utility plans prior to Pre-Con being scheduled.
- ☐ **Final Approval from Development Services is required** prior to scheduling a Pre-Con.

## Request for Appeal of Staff Decision

1. **Appeal of Staff Decision:** The Commission shall hear and decide appeals by an applicant of the final decision made by the Development Services Manager when it is alleged that the Development Services Manager did not properly apply this section 7101.6, did not consider all of the relevant facts presented, made an error of fact or law, abused discretion or acted arbitrarily and capriciously in the interpretation or enforcement of the ACHD Policy Manual.
  - a. **Filing Fee:** The Commission may, from time to time, set reasonable fees to be charged the applicant for the processing of appeals, to cover administrative costs.
  - b. **Initiation:** An appeal is initiated by the filing of a written notice of appeal with the Secretary and Clerk of the District, which must be filed within ten (10) working days from the date of the decision that is the subject of the appeal. The notice of appeal shall refer to the decision being appealed, identify the appellant by name, address and telephone number and state the grounds for the appeal. The grounds shall include a written summary of the provisions of the policy relevant to the appeal and/or the facts and law relied upon and shall include a written argument in support of the appeal. The Commission shall not consider a notice of appeal that does not comply with the provisions of this subsection.
  - c. **Time to Reply:** The Development Services Manager shall have ten (10) working days from the date of the filing of the notice of appeal to reply to the notice of the appeal, and may during such time meet with the appellant to discuss the matter, and may also consider and/or modify the decision that is being appealed. A copy of the reply and any modifications to the decision being appealed will be provided to the appellant prior to the Commission hearing on the appeal.
  - d. **Notice of Hearing:** Unless otherwise agreed to by the appellant, the hearing of the appeal will be noticed and scheduled on the Commission agenda at a regular meeting to be held within thirty (30) days following the delivery to the appellant of the Development Services Manager's reply to the notice of appeal. A copy of the decision being appealed, the notice of appeal and the reply shall be delivered to the Commission at least one (1) week prior to the hearing.
  - e. **Action by Commission:** Following the hearing, the Commission shall either affirm or reverse, in whole or part, or otherwise modify, amend or supplement the decision being appealed, as such action is adequately supported by the law and evidence presented at the hearing.

## Request for Reconsideration of Commission Action

1. **Request for Reconsideration of Commission Action:** A Commissioner, a member of ACHD staff or any other person objecting to any final action taken by the Commission may request reconsideration of that action, provided the request is not for a reconsideration of an action previously requested to be reconsidered, an action whose provisions have been partly and materially carried out, or an action that has created a contractual relationship with third parties.

- a. Only a Commission member who voted with the prevailing side can move for reconsideration, but the motion may be seconded by any Commissioner and is voted on by all Commissioners present.

If a motion to reconsider is made and seconded it is subject to a motion to postpone to a certain time.

- b. The request must be in writing and delivered to the Secretary of the Highway District no later than 11:00 a.m. 2 days prior to the Commission's next scheduled regular meeting following the meeting at which the action to be reconsidered was taken. Upon receipt of the request, the Secretary shall cause the same to be placed on the agenda for that next scheduled regular Commission meeting.
- c. The request for reconsideration must be supported by written documentation setting forth new facts and information not presented at the earlier meeting, or a changed situation that has developed since the taking of the earlier vote, or information establishing an error of fact or law in the earlier action. The request may also be supported by oral testimony at the meeting.
- d. If a motion to reconsider passes, the effect is the original matter is in the exact position it occupied the moment before it was voted on originally. It will normally be returned to ACHD staff for further review. The Commission may set the date of the meeting at which the matter is to be returned. The Commission shall only take action on the original matter at a meeting where the agenda notice so provides.
- e. At the meeting where the original matter is again on the agenda for Commission action, interested persons and ACHD staff may present such written and oral testimony as the President of the Commission determines to be appropriate, and the Commission may take any action the majority of the Commission deems advisable.
- f. If a motion to reconsider passes, the applicant may be charged a reasonable fee, to cover administrative costs, as established by the Commission.





## CITY OF STAR

# LAND USE STAFF REPORT

**TO:** Mayor & Council

**FROM:** Shawn L. Nickel, Planning Director & Zoning Administrator *Shawn L. Nickel*

**MEETING DATE:** **October 12, 2021 – PUBLIC HEARING - Tabled from September 7, 2021**

**FILE(S) #:** PP-21-12 Preliminary Plat for Moon Valley Townhomes  
DA-21-13-MOD Development Agreement Modification  
PR-21-11 Private Street

### OWNER/APPLICANT/REPRESENTATIVE

**Property Owner:**

Ball Real Estate Investments, LLC  
7033 E. Greenway Parkway, Ste. 300  
Scottsdale, Arizona 85254

**Property Owner:**

Sundance Investments, LP  
3405 E. Overland Road, Ste. 150  
Meridian, Idaho 83642

**Representative:**

Wendy Shrief, JUB Engineers, Inc.  
2760 W. Excursion Lane Ste. 400  
Meridian, Idaho 83642

**Applicant:**

M3 ID Moon Valley, Inc.  
1087 W. River St. Suite 310  
Boise, Idaho 83702

### REQUEST

**Request:** The Applicant is seeking approval of a Preliminary Plat for a proposed residential subdivision in the Mixed-Use zone consisting of 156 residential lots and 1 common lot, a Development Agreement Modification and Private Streets. The property is located 8323 W. Moon Valley Road in Star, Idaho, and consists of 12.06 acres with a proposed density of 12.94 units per acre.

### PROPERTY INFORMATION

**Property Location:** The subject property is generally located on the western end of W. Moon Valley Road and adjacent to Highway 16. Ada County Parcel No's. S0416110105, S0416110207 & S0416120646.

**Existing Site Characteristics:** The property is currently vacant.

**Irrigation/Drainage District(s):** - Little Pioneer Ditch, P.O. Box 70, Star, Idaho 83669  
- Drainage District #2, c/o Sawtooth Law Offices, 1101 W. River Street Ste. 110, Boise, Idaho 83707

**Flood Zone:** The development is located in a special flood hazard zone per FEMA FIRM panel #16001C0130 H and 16601C0140 H. Base flood elevation in the AE zone is 2491-2494.

**Special On-Site Features:**

- ✧ Areas of Critical Environmental Concern – No known areas.
- ✧ Evidence of Erosion – No known areas.
- ✧ Fish Habitat – No known areas.
- ✧ Mature Trees – No.
- ✧ Riparian Vegetation – No known areas.
- ✧ Steep Slopes – No.
- ✧ Stream/Creek – None.
- ✧ Unique Animal Life – No unique animal life has been identified.
- ✧ Unique Plant Life – No unique plant life has been identified.
- ✧ Unstable Soils – No known issues.
- ✧ Wildlife Habitat – No wildlife habitat has been developed or will be destroyed.
- ✧ Historical Assets – No historical assets have been observed.

|                                 |
|---------------------------------|
| <b>APPLICATION REQUIREMENTS</b> |
|---------------------------------|

|                                   |                    |
|-----------------------------------|--------------------|
| Pre-Application Meeting Held      | September 16, 2020 |
| Neighborhood Meeting Held         | February 24, 2021  |
| Application Submitted & Fees Paid | April 20, 2021     |
| Application Accepted              | May 10, 2021       |
| Residents within 300' Notified    | August 17, 2021    |
| Agencies Notified                 | May 10, 2021       |
| Legal Notice Published            | August 19, 2021    |
| Property Posted                   | August 27, 2021    |

|                |
|----------------|
| <b>HISTORY</b> |
|----------------|

|                |   |
|----------------|---|
| April 16, 2019 | Council approved a Comprehensive Plan Map Amendment (CPA-19-01) and Rezone from Residential (R-2) to Mixed Use (RZ-18-07) with a Development Agreement Modification (DA-19-02-MOD). |
|----------------|---|

|  |
|--|
| <b>SURROUNDING ZONING/COMPREHENSIVE PLAN MAP/LAND USE DESIGNATIONS</b> |
|--|

|                      | <b>Zoning Designation</b> | <b>Comp Plan Designation</b>      | <b>Land Use</b>                           |
|----------------------|---------------------------|-----------------------------------|---|
| <b>Existing</b>      | Mixed Use                 | Commercial                        | Vacant                                    |
| <b>Proposed</b>      | Mixed Use                 | Commercial                        | Residential/Townhomes                     |
| <b>North of site</b> | RUT/R-1/C-1               | Commercial                        | Single Family<br>Residential/Vacant       |
| <b>South of site</b> | Mixed Use (MU-DA)         | Mixed Use (MU)                    | Approved Moon Valley<br>Subdivision       |
| <b>East of site</b>  | RUT                       | Mixed Use (MU)                    | Single Family<br>Residential/Agricultural |
| <b>West of site</b>  | Commercial (C-2)          | Commercial/Industrial<br>Corridor | Vacant/Highway<br>16/Agricultural         |

|  |
|--|
| <b>ZONING ORDINANCE STANDARDS / COMPREHENSIVE PLAN</b> |
|--|

**UNIFIED DEVELOPMENT CODE:**

**8-3B-1: ZONING DISTRICTS AND PURPOSE ESTABLISHED:**

MU MIXED USE DISTRICT: To provide for a mixture of uses which may, at the sole discretion of the Council, include office, commercial, and/or residential depending upon the specific comprehensive plan area designated as Mixed Use. Development within this zone is to proceed through the PUD process unless a development agreement has already been executed for the particular property. Identifying areas for mixed-use development has two objectives. The first objective is to give the city a better tool to manage the type of developments through the planned unit development and/or the Development Agreement process. The second objective is that this zone may allow the development community to be more innovative in design and placement of structures subject to Council review and approval. Rezoning within this land use designation is to be strictly monitored by the city to assure that the Mixed-Use areas are not being used simply to justify high density residential use. Residential uses may be part of an overall mixed-use development that includes a non-residential component and may not exceed 30% of the overall size of the development.

DA DEVELOPMENT AGREEMENT: This designation, following any zoning designation noted on the official zoning map of the city (i.e., C-2-DA), indicates that the zoning was approved by the city with a development agreement, with specific conditions of zoning.

**8-3A-3: USES WITHIN ZONING DISTRICTS**

The following table lists principal permitted (P), accessory uses (A), conditional (C), or prohibited (N) uses.

| <b><u>ZONING DISTRICT USES</u></b> | <b>A</b> | <b>R</b> | <b>MU</b> |
|------------------------------------|----------|----------|-----------|
| Accessory structure                | A        | A        | C/P       |
| Dwelling:                          |          |          |           |
| Multi-family 1                     | N        | C        | C         |
| Secondary 1                        | A        | A        | C         |
| Single-family attached             | N        | C        | C         |
| Single-family detached             | P        | P        | C         |
| Two-family duplex                  | N        | P        | C         |

#### 8-3A-4: ZONING DISTRICT DIMENSIONAL STANDARDS:

| Zoning District | Maximum Height  | Minimum Yard Setbacks<br>Note Conditions  |                      |  |             |
|-----------------|-----------------|---|----------------------|--|-------------|
|                 | Note Conditions | Front (1)   | Rear                 | Interior Side                              | Street Side |
| MU              | 35'             | For MU and CBD – Unless otherwise approved by the Council as a part of a PUD or Development Agreement, all residential buildings shall follow the residential setbacks shown in this table based upon the project density and all other buildings shall follow setbacks for the C-2 zone (3). |                      |  |             |
| R-12 and higher | 35'             | 15' to living area 20' to garage  | 15' 4' if alley load | 5' for single story<br>10' for multi-story | 20'         |

#### Notes:

1. Front yard setback shall be measured from the face of the garage to the face of the sidewalk, allowing for 20' of parking on the driveway without overhang onto the sidewalk.
2. Interior side yard setbacks for lots with 50' or less of lot width shall be allowed 5' interior side yard setbacks for one and two-story structures.
3. All setbacks in the M-U zone shall be a minimum of 15' when adjacent to a residential use or zone.

### **8-4D-3: STANDARDS (PRIVATE STREETS):**

All private streets shall be designed and constructed to the following standards:

#### **A. Design Standards:**

1. Easement: The private street shall be constructed on a perpetual ingress/egress easement or a single platted lot (with access easement) that provides access to all applicable properties.
2. Connection Point: Where the point of connection of the private street is to a public street, the private street shall be approved by the transportation authority.
3. Emergency Vehicle: The private street shall provide sufficient maneuvering area for emergency vehicles as determined and approved by the Star Fire District.
4. Gates: Gates or other obstacles shall not be allowed, unless approved by Council through a Planned Unit Development or Development Agreement.

#### **B. Construction Standards:**

1. Obtain approval from the county street naming committee for a private street name(s);
2. Contact the transportation authority to install an approved street name sign that complies with the regulations of the county street naming ordinance;
3. Roadway and Storm Drainage: The private street shall be constructed in accord with the roadway and storm drainage standards of the transportation authority or as approved by the city of Star based on plans submitted by a certified engineer.
4. Street Width: The private street shall be constructed within the easement and shall have a travel lane that meets ACHD width standards for the City of Star, or as determined by the Council and Star Fire District.
5. Sidewalks: A five foot (5') attached or detached sidewalk shall be provided on one side of the street in commercial districts. This requirement may be waived if the applicant can demonstrate that an alternative pedestrian path exists.
6. Fire Lanes: All drive aisles as determined by the Star Fire District to be fire lanes, shall be posted as fire lanes with no parking allowed. In addition, if a curb exists next to the drive aisle, it shall be painted red.
7. No building permit shall be issued for any structure using a private street for access to a public street until the private street has been approved.

C. The applicant or owner shall establish an on-going maintenance fund through the Owner's association with annual maintenance dues to ensure that funds are available for future repair and maintenance of all private streets. This shall be a requirement in a development agreement



and/or as part of a planned unit development. A reserve account condition shall be included in the recorded CC&R's and shall be provided to the City for review. The condition of approval shall include the following:

1. Private Road Reserve Study Requirements.

- a. At least once every three years, the board shall cause to be conducted a reasonably competent and diligent visual inspection of the private road components that the association is obligated to repair, replace, restore, or maintain as part of a study of the reserve account requirements of the common interest development, if the current replacement value of the major components is equal to or greater than one-half of the gross budget of the association, excluding the association's reserve account for that period. The board shall review this study, or cause it to be reviewed, annually and shall consider and implement necessary adjustments to the board's analysis of the reserve account requirements as a result of that review.
- b. The study required by this section shall at a minimum include:
  - i. Identification of the private road components that the association is obligated to repair, replace, restore, or maintain.
  - ii. Identification of the probable remaining useful life of the components identified in paragraph (1) as of the date of the study.
  - iii. An estimate of the cost of repair, replacement, restoration, or maintenance of the components identified in paragraph (1).
  - iv. An estimate of the total annual contribution necessary to defray the cost to repair, replace, restore, or maintain the components identified in paragraph (1) during and at the end of their useful life, after subtracting total reserve funds as of the date of the study.
  - v. A reserve funding plan that indicates how the association plans to fund the contribution identified in paragraph (4) to meet the association's obligation for the repair and replacement of all private road components.
- c. A copy of all studies and updates shall be provided to the City, to be included in the development application record.

**8-4D-4: REQUIRED FINDINGS (PRIVATE STREETS):**

In order to approve the application, the administrator and/or Council shall find the following:

- A. The design of the private street meets the requirements of this article;
- B. Granting approval of the private street would not cause damage, hazard, or nuisance, or other detriment to persons, property, or uses in the vicinity; and
- C. The use and location of the private street shall not conflict with the comprehensive plan and/or the regional transportation plan.

## **8-4E-2: STANDARDS FOR COMMON OPEN SPACE AND SITE AMENITY REQUIREMENTS:**

A. Open Space and Site Amenity Requirement (see also Chapter 8 "Architectural Review"):

1. The total land area of all common open space shall equal or exceed fifteen percent (15%) of the gross land area of the development. Ten percent (10%) of that area shall be usable open space.
2. Each development is required to have at least one site amenity.
3. One additional site amenity shall be required for each additional twenty (20) acres of development area, plus one additional amenity per 75 residential units.
4. Developments with a density of less than 2 dwelling units per acre may request a 50% reduction in total required open space to the Council.
5. For multi-family developments, see Section 8-5-20 for additional standards.

B. Qualified Open Space: The following may qualify to meet the common open space requirements:

1. Any open space that is active or passive in its intended use, and accessible or visible by all residents of the development, including, but not limited to:
  - a. Open grassy area of at least fifty feet by one hundred feet (50' x 100') in area;
  - b. Qualified natural areas;
  - c. Ponds or water features where active fishing, paddle boarding or other activities are provided (50% qualifies towards total required open space, must be accessible by all residents to qualify.) ponds must be aerated;
  - d. A plaza.
2. Additions to a public park or other public open space area.
3. The buffer area along collector and arterial streets may be included in required overall common open space for residential subdivisions.
4. Parkway along local residential streets with detached sidewalks that meet all the following standards may count toward the common open space requirement:
  - a. The parkway is a minimum of eight feet (8') in width from street curb to edge of sidewalk and includes street trees as specified otherwise herein.
  - b. Except for alley accessed dwelling units, the area for curb cuts to each residential lot or common driveway shall be excluded from the open space calculation. For purposes of this calculation, the curb cut area shall be a minimum area of twenty-six feet (26') by the width of the

parkway.

c. Stormwater detention facilities do not qualify to meet the common area open space requirements, unless all of the following is met:

1. Must be at least fifty feet by one hundred feet (50' x 100') in area;
2. Specifically designed as a dual use facility, as determined by the administrator, to include minimal slopes, grass throughout, and guarantee of water percolation within 24 hours of storm event.
3. Is located in a development that has a second usable open space area that contains a qualified site amenity as herein defined.

5. Visual natural space, including open ditches, wetlands, slopes or other areas that may not be readily accessible to residents, and is provided with open style fencing, may qualify for up to 20% of the required open space total.

C. Qualified Site Amenities: Qualified site amenities shall include, but not be limited to, the following:

1. Clubhouse;
2. Fitness facilities, indoors or outdoors;
3. Public art;
4. Picnic area; or
5. Recreation amenities:
  - a. Swimming pool.
  - b. Children's play structures.
  - c. Sports courts.
  - d. Additional open space in excess of 5% usable space.
  - e. RV parking for the use of the residents within the development.
  - f. School and/or Fire station sites if accepted by the district.
  - g. Pedestrian or bicycle circulation system amenities meeting the following requirements:
    - (1) The system is not required for sidewalks adjacent to public right of way;
    - (2) The system connects to existing or planned pedestrian or bicycle routes outside the development; and
    - (3) The system is designed and constructed in accord with standards set forth by the city of Star;

D. Location: The common open space and site amenities shall be located on a common lot or an area with a common maintenance agreement.

E. Maintenance:

1. All common open space and site amenities shall be the responsibility of an owners' association for the purpose of maintaining the common area and improvements thereon.

#### **8-6A-7: REQUIRED FINDINGS:**

In consideration of a preliminary plat or combined preliminary and final plat, the decision-making body shall make the following findings:

A. The plat is in conformance with the comprehensive plan;

B. Public services are available or can be made available and are adequate to accommodate the proposed development;

C. There is public financial capability of supporting services for the proposed development;

D. The development will not be detrimental to the public health, safety or general welfare; and

E. The development preserves significant natural, scenic or historic features.

#### **COMPREHENSIVE PLAN:**

##### **8.2.3 Land Use Map Designations:**

###### **Mixed Use:**

Generally suitable for a mixture of uses which may, at the sole discretion of the Council, include office, commercial, and/or residential depending upon the specific area designated as Mixed Use. See Mixed Use Implementation Policies for specific criteria. Development within this land use designation is to proceed through the PUD and/or development agreement process. Identifying areas for mixed-use development has two objectives. The first objective is to give the city a better tool to manage the type of developments through the planned unit development and/or the Development Agreement process. The second objective is that this land use designation will allow the development community to be more innovative in design and placement of structures. Development design guidelines should also be established to guide development within mixed-use areas. Rezoning within this land use designation is to be strictly monitored by the city to assure that the Mixed-Use areas are not being used simply to justify high density residential use.

##### **8.3 Goal:**

Encourage the development of a diverse community that provides a mix of land uses, housing types, and a variety of employment options, social and recreational opportunities, and where possible, an assortment of amenities within walking distance of residential development.

#### 8.4 Objectives:

- Implement the Land Use Map and associated policies as the official guide for development.
- Manage urban sprawl in order to minimize costs of urban services and to protect rural areas.
- Encourage land uses that are in harmony with existing resources, scenic areas, natural wildlife areas, and surrounding land uses.

#### 8.5.3 Policies Related Mostly to the Urban Residential Planning Areas:

A. The Neighborhood Residential Land Use is to encourage urban style development densities to limit urban sprawl.

B. Low densities within the Neighborhood Residential Land Use are to be designed within the floodplain, ridgeline developable areas, hillside developable areas and where new residential lots are proposed adjacent to existing residential lots of one acre and larger where

#### 8.5.7 Policies Related Mostly to the Mixed-Use Planning Areas

A. Council, at their sole discretion, shall determine what mix of uses are appropriate for any mixed-use area considering existing property owners rights.

B. Development within the Mixed-Use Designation is to proceed through the CUP, PUD, and/or Development Agreement process, and a concept plan must be included with any such proposed use.

C. In general, mixed use areas along state highways should be predominantly commercial with a very minor component of residential unless the residential is placed on upper floors as part of a mixed-use building.

D. Mixed-use areas along state and U.S. Highways where direct access to the state highway is prohibited, like along State Highway 16 between State Highway 44 and US Highway 20/26, should be predominately residential with a minor component of neighborhood commercial, or light industrial if sufficient roadway access, by means of backage or other roads, to the State Highway is provided.

E. Mixed-use areas located between commercial and residential land use designations are to provide a compatible transition between the higher intensity use of commercial and the lower intensity use of Neighborhood Residential. Uses for these mixed-use areas could include multi-family housing and or office related uses if determined by the Council through the public hearing process, to be appropriate.



#### 8.5.9 Additional Land Use Component Policies:

- Encourage flexibility in site design and innovative land uses.
- Encourage landscaping to enhance the appearance of subdivisions, structures, and parking areas.
- Require more open space and trees in subdivisions.
- Work with Ada County Highway District (ACHD), Canyon Highway District #4 (CHD4), and Idaho Department of Transportation (ITD) for better coordination of roadway and access needs.
- Support well-planned, pedestrian-friendly developments.
- Dark sky provision should be adopted within the code to assure down style lighting in all developments and Star should consider joining the International Dark Sky Association.
- The City should utilize the 2018 Treasure Valley Tree Selection Guide when requiring trees within developments.

#### 18.4 Implementation Policies:

E. Development Agreements allow the city to enter into a contract with a developer upon rezoning. The Development Agreement may provide the city and the developer with certain assurances regarding the proposed development upon rezoning.

### PROJECT OVERVIEW

#### PRELIMINARY PLAT:

The Preliminary Plat submitted contains 156 single family residential lots, and 1 common area lot on 12.06 acres for a density of 12.94 dwelling units per acre. The residential lots range in size from 1,164 square feet to 1,357 square feet, with an average lot size of 1,164 square feet. Each townhome will be located on individual lots to allow for private home ownership. All streets are proposed to be private streets and must be built to ACHD standards. The Applicant is also requesting two (2) gates at the front of the property. **The submitted preliminary plat indicates street widths at 25-26 feet, well below the minimum required by Section 8-4D-34B (4) of the UDC. The applicant is proposing the development to gated on both sides of the main entrance. All private streets and gates must be approved by the Council and Fire District and shall meet all Star Fire District standards.**

The development will be serviced by Star Sewer & Water District with connections that are on the southern boundary of the proposed development. The development will also connect to existing pressure irrigation.

The preliminary plat indicates that the development will contain a total of 4.40 acres (36.5%) total open space within the one common lot. The development meets the minimum of 15% open space, 10% usable space required by Code.

The current Unified Development Code, Section 8-4E-2 requires a development of this size to have a minimum of 3 site amenities. The applicant is proposing a clubhouse; pool, firepit and large gazebo with a BBQ. There will also be a walking path around the central open area. These amenities satisfy the code requirement for development amenities.

The development will be accessed from South Herons Flight Lane which is a public road. There is a secondary, emergency access on the southwest corner of the property. Applicant is proposing this to be gated.

#### **ADDITIONAL DEVELOPMENT FEATURES:**

- Private Streets  
The development is proposed to contain private streets. As was approved with the rest of the Moon Valley development, all private streets should be built to ACHD roadway standards, including a minimum of 33 feet of improved width. The applicant may request approval through the development agreement for alternatives to this requirement. However, Star Fire District must approve the use of these streets as proposed. **The Fire District has requested that the applicant meet with the District to discuss the planned access.**
- Sidewalks  
Sidewalks are proposed at five-foot (5') widths and will be attached throughout this portion of the subdivision.
- Lighting  
Streetlights shall reflect the "Dark Sky" criteria with all lighting. The same streetlight design shall continue throughout the entire development.
- Street Names and Addressing  
Street names and addressing must be approved by the Ada Street Naming Committee prior to signature of final plat.
- Landscaping - As required by the Unified Development Code, Chapter 8, Section 8-8C-2-M(2) Street Trees; A minimum of one street tree shall be planted for every thirty-five (35) linear feet of street frontage. **The applicant should request a waiver of this requirement through the Development Agreement for an alternative landscaping plan for the omission of the street trees.** The applicant shall use "Treasure Valley Tree Selection Guide", as adopted by the Unified Development Code.

- Setbacks – The applicant has requested special setbacks for the attached units with zero-lot-lines.
- Parking – The development requires a minimum of 39 visitor parking spaces and the applicant is proposing 55, satisfying Section 8-4B-3 of the Unified Development Code.

## **DEVELOPMENT AGREEMENT - MODIFICATION**

Through the Development Agreement process, the applicant is proposing to work with the City and neighboring property owners to provide further assurances that the development will be built as presented and/or modified by the Council through the review process. Items that should be considered by the applicant and Council include the following:

- Density;
- Percentage of Residential in Mixed Use Zone;
- ITD Proportionate Share Fees;
- Private Road Maintenance;
- Private Road Study Every Three (3) Years;
- Emergency Access;

### **AGENCY RESPONSES**

|                              |                 |
|------------------------------|-----------------|
| Star Fire District           | August 30, 2021 |
| Keller and Associates        | May 28, 2021    |
| ITD                          | June 16, 2021   |
| ACHD                         | August 4, 2021  |
| Central District Health Dept | May 12, 2021    |
| DEQ                          | May 21, 2021    |

### **PUBLIC RESPONSES**

Staff has received multiple letters from concerned neighbors regarding the development.

### **STAFF ANALYSIS & RECOMMENDATIONS**

Staff has concerns with the proposal for additional residential development on this mixed-use zoned property as it relates to the Comprehensive Plan and Unified Development Code. Regarding Mixed-Use, the Comprehensive Plan states the following within the Goals and Policies:

### 8.2.3 Land Use Map Designations:

#### *Mixed Use:*

Generally suitable for a mixture of uses which may, at the sole discretion of the Council, include office, commercial, and/or residential depending upon the specific area designated as Mixed Use.

### 8.5.7 Policies Related Mostly to the Mixed-Use Planning Areas

A. Council, at their sole discretion, shall determine what mix of uses are appropriate for any mixed-use area considering existing property owners rights.

### 8.5.9 Additional Land Use Component Policies:

- Work with Ada County Highway District (ACHD), Canyon Highway District #4 (CHD4), and Idaho Department of Transportation (ITD) for better coordination of roadway and access needs.

Regarding the Unified Development Code, specifically the purpose statement for Mixed Use, the Code states:

#### *8-3B-1: ZONING DISTRICTS AND PURPOSE ESTABLISHED:*

MU MIXED USE DISTRICT: To provide for a mixture of uses which may, at the sole discretion of the Council, include office, commercial, and/or residential depending upon the specific comprehensive plan area designated as Mixed Use. Development within this zone is to proceed through the PUD process unless a development agreement has already been executed for the particular property. Identifying areas for mixed-use development has two objectives. The first objective is to give the city a better tool to manage the type of developments through the planned unit development and/or the Development Agreement process. The second objective is that this zone may allow the development community to be more innovative in design and placement of structures subject to Council review and approval. Rezoning within this land use designation is to be strictly monitored by the city to assure that the Mixed-Use areas are not being used simply to justify high density residential use. Residential uses may be part of an overall mixed-use development that includes a non-residential component and may not exceed 30% of the overall size of the development.

This last portion of property previously zoned Mixed Use by the Council increases the total residential use in this entire development to 100% residential. The purpose statement for Mixed Use clearly intends for this zoning designation to have some non-residential uses. Council has voiced this concern during past public hearings on previous phases (see attached Council minutes from 4-16-19). In addition, the Comprehensive Plan Future Land Use Map designates

this property as Commercial. And while this applicant is not requesting a rezone, where findings regarding Comprehensive Plan compliance is not necessary, and the existing zoning designation and existing Development Agreement does allow for this use, staff finds it hard to make the recommendation that Findings of Fact #1 for a Preliminary Plat can be met.

Therefore, Council should consider the entire record and testimony presented at their scheduled public hearing prior to rendering its decision on the matter. Should the Council vote to approve the applications, either as presented or with added conditions of approval, Council shall direct staff to draft findings of fact and conclusions of law for the Council to consider at a future date. Staff has included proposed Conditions of Approval in the event that Council approves the request.

## FINDINGS

The Council may **approve**, **conditionally approve**, **deny** or **table** this request. In order to approve these applications, the Unified Development Code requires that Council must find the following:

### ANNEXATION/REZONE FINDINGS:

1. The map amendment complies with the applicable provisions of the Comprehensive Plan.

*The purpose of the Star Comprehensive Plan is to promote the health, safety, and general welfare of the people of the City of Star and its Impact Area. Some of the prime objectives of the Comprehensive Plan include:*

- ✓ *Protection of property rights.*
- ✓ *Adequate public facilities and services are provided to the people at reasonable cost.*
- ✓ *Ensure the local economy is protected.*
- ✓ *Encourage urban and urban-type development and overcrowding of land.*
- ✓ *Ensure development is commensurate with the physical characteristics of the land.*

*The goal of the Comprehensive Plan for Land Use is to encourage the development of a diverse community that provides a mixture of land uses, housing types, and a variety of employment options, social and recreational opportunities, and where possible provides an assortment of amenities within walking distance of a residential development. The City must find compliance with the Comprehensive Plan.*

2. The map amendment complies with the regulations outlined for the proposed district, specifically, the purposes statement.

*The City must find that the proposal complies with the proposed district and purpose statement. The purpose of the residential districts is to provide regulations and districts for various residential neighborhoods with gross densities in compliance with the intent of the Comprehensive Plan designation. Housing shall be single family detached unless*



*approved with a PUD or development agreement. Connection to municipal water and sewer facilities are required for all subdivision and lot split applications in all districts exceeding one dwelling unit per acre. Private streets may be approved in this district for access to newly subdivided or split property.*

3. The map amendment shall not be materially detrimental to the public health, safety, and welfare; and

*The City must find that there is no indication from the material submitted by any political agency stating that this annexation and zoning of this property will be materially detrimental to the public health, safety or welfare.*

4. The map amendment shall not result in an adverse impact upon the delivery of services by any political subdivision providing public services within the city including, but not limited to, school districts.

*The City must find that it has not been presented with any information from agencies having jurisdiction that public services will be adversely impacted other than traffic, which will continue to be impacted as the City grows.*

5. The annexation is in the best interest of the city.

*The City must find that this annexation is reasonably necessary for the orderly development of the City.*

#### **PRELIMINARY PLAT FINDINGS:**

1. The plat is in compliance with the Comprehensive Plan.

*The City must find that this Plat follows designations, spirit and intent of the Comprehensive Plan regarding residential mixed-use developments and meets several of the objectives of the Comprehensive Plan such as:*

- 1. Designing development projects that minimize impacts on existing adjacent properties, and*
- 2. Managing urban sprawl to protect outlying rural areas.*

2. Public Services are available or can be made available and are adequate to accommodate the proposed development.

*The City must find that Agencies having jurisdiction on this parcel were notified of this action, and that it has not received notice that public services are not available or cannot be made available for this development.*

3. There is public financial capability of supporting services for the proposed development;

*The City must find that they have not been notified of any deficiencies in public financial capabilities to support this development.*

4. The development will not be detrimental to the public health, safety or general welfare;

*The City must find that it has not been presented with any facts stating this Preliminary Plat will be materially detrimental to the public health, safety and welfare. Residential uses are a permitted use within the zone.*

5. The development preserves significant natural, scenic or historic features;  
*The City must find that there are no known natural, scenic, or historic features that have been identified within this Preliminary Plat.*

**PRIVATE ROAD FINDINGS:**

- A. The design of the private street meets the requirements of this article;  
*The City must find that the proposed private road meets the design standards in the Code.*
- B. Granting approval of the private street would not cause damage, hazard, or nuisance, or other detriment to persons, property, or uses in the vicinity:

*The City must find that it has not been presented with any facts stating this private road will cause damage, hazard or nuisance, or other detriment to persons, property or uses in the vicinity.*

- C. The use and location of the private street shall not conflict with the comprehensive plan and/or the regional transportation plan.

*The City must find that the use is not in conflict with the comprehensive plan and/or regional transportation plan.*

*Upon granting approval or denial of the application, the Council shall specify:*

1. The Ordinance and standards used in evaluating the application;
2. The reasons for recommending approval or denial; and
3. The actions, if any, that the applicant could take to obtain approval.

|                               |
|-------------------------------|
| <b>CONDITIONS OF APPROVAL</b> |
|-------------------------------|

1. The approved Preliminary Plat for Moon Valley Townhomes shall comply with all statutory requirements of applicable agencies and districts having jurisdiction in the City of Star.
2. **All private streets shall have a minimum street width of 33' and shall be constructed to ACHD standards, unless otherwise approved by the Council and Star Fire District.**
3. **Applicant shall provide approval from Ada County on private street names and or addressing plan prior to signing the final plat.**

4. All approvals relating to floodplain/floodway issues and requirements shall be completed and approved by the City Flood Administrator **prior to submittal of the final plat.**  
**Construction of any kind is prohibited prior to approval of a Floodplain application.**
5. The property with the approved Preliminary Plat shall be satisfactorily weed abated at all times, preventing a public nuisance, per Star City Code.
6. Street trees shall be installed per Chapter 8, Section 8-8C-2-M(2) Street Trees.
7. All signed Irrigation District Agreements with the Irrigation Districts shall be provided to the City of Star with each subsequent Final Plat application.
8. Pressurized irrigation systems shall comply with the Irrigation District(s) and the City of Star Codes. Plans for pressurized irrigation systems shall be submitted to, and approved by the City of Star Engineer, prior to installation.
9. A plat note supporting the "Right to Farm Act" as per Idaho Code Title 22, Chapter 45, shall be shown on the Final Plat.
10. A copy of the CC&R's shall be submitted to the City of Star at Final Plat.
- 11. A letter from the US Postal Service shall be given to the City at Final Plat stating the subdivision is in compliance with the Postal Service.**
12. A form signed by the Star Sewer & Water District shall be submitted to the City prior to the signature of the Final Plat stating that all conditions of the District have been met.
13. A plat note shall state that development standards for residential development shall comply with the effective building and zoning requirements at time of building permit issuance, unless amended in the Development Agreement or CUP conditions.
- 14. Streetlights shall comply with the Star City Code and shall be of the same design throughout the entire subdivision. Streetlights shall be continuous throughout the subdivision and shall be maintained by the Homeowners Association. Streetlights shall be installed and energized prior to issuing any building permits. Design shall follow Code with requirements for light trespass and "Dark Skies" lighting. Client has submitted a plan and Staff would like to work with the applicant on locations of streetlights. Applicant has not submitted a streetlight design/cutsheet. Streetlight design must be submitted and approved by Staff prior to signing the final plat.**
15. Development standards for single family residential units shall comply with effective building and zoning requirements at time of building permit issuance, or as approved through the Development Agreement or as stated herein.
16. All common areas shall be maintained by the Homeowners Association.
17. The applicant shall provide a sign, to be located at all construction entrances, indicating the rules for all contractors that will be working on the property starting at grading and running through home sales that addresses items including but not limited to dust, music, dogs, starting/stopping hours for contractors (7a.m. start time). **Sign shall be approved by the City prior to start of construction.**
18. A sign application is required for any subdivision signs.
19. Any additional Condition of Approval as required by Staff and City Council.

|                         |
|-------------------------|
| <b>COUNCIL DECISION</b> |
|-------------------------|

The Star City Council \_\_\_\_\_ File # PP-21-13/DA-21-12 MOD/PR-21-11for Moon  
Valley Townhomes on \_\_\_\_\_, 2020.

## City Council Minutes from 4-16-19

### Discussion from April 16, 2019 Moon Valley Subdivision Regarding Mixed Use

1:58:17 of Tape

MK - Mr. Mayor, if I can continue. So, Mr. Tate, also, we discussed the, my concerns about signing a blank check if you will on the mixed use area on the northwestern corner, and would you be willing to put into the development agreement, or amendment, that when you bring forward a proposal for that that you would be willing to follow our updated Comprehensive Plan, coming soon to a City near you, and also our updated Development Code, or whatever that might be at the time?

MT – We’ve submitted the Development Agreement and Conditional Use permit not knowing what’s coming, I do not feel comfortable in saying that I will agree to something that I haven’t seen in the future, so I respectfully, I would like to keep it with our development agreement and conditional use permit just cause its an unknown quantity and not know what’s going on.....

TC - So let’s talk about this mixed-use thing here. I know you made a comment and I’ve heard this comment many times, about no one’s going to want to drive down to Palmer and come back on to highway 44, however, if you know the citizens of Star and they drive 7 miles anyway to get to anyplace to go, so if we have shops and stuff in there and we made that a priority to get it done, your going to have people that are going to visit that, you’ll have 200 folks or whatever here that would visit those things. I would personally like to see..., this is an important corner, this is a vital corner, our City is upside down and we’ve said this many times and our residential vs commercial projects, and were not viable yet, you know as far as tax base goes, so were taking a whole big section of this and turning it into residential... That corner right there, that little stub, that you have going north there right underneath plan, I’d like to see something done different in there with mixed use and do some shops and boardwalks and stuff along that lake to make it attractive and make is some place where people want to come,

MT- Right, and that’s why we pulled it all the way back to the lake to leave that as an opportunity

TC – So your option, your idea is for that section on the side of that to be mixed use right there or is that going to be open space?

MT – Right here?

TC – Yes.

MT – This is part of that mixed use, this area that we have in our preliminary plat, so its kind of wraps that corner.

TC - So it will wrap around that section there and go up?

MT – All the way to Moon Valley Road, yep. If you would actually go to the next slide, that, yeah here you go, so here’s that lake, so the application actually includes all this property that goes up to Moon Valley Road.

TC - And that empty spot on the right there is not your property?

MT - And so to your point, about wanting to see those uses, you know I’d like to see those uses too, just from a project standpoint, commercial property is worth more, and we’ve done all types of

development so that's not out of our bailiwick to do that so at this point we don't have enough information on the access to be able to tell what's realistic in there or not.

KN – I think you have some good ideas as far as light office, maybe a retirement center, Dr.'s office..

MT – I think that's a good idea..

KN – Stuff like that I think people would drive out of their way....

MT - And keep in mind, we can do some of that here, and there's all this property on the north side that's going to have a lot more visibility to traffic that could come in and be a lot easier to get in and maybe if some big application came in we would at least be able to have a right in right out directly on to state street in that area.

KN – I think that what your hearing from the Council is that were pretty intent on seeing that develop as commercial use, I don't think your going to find an approval tonight without some sort of agreement that that is what is going to turn into, so I don't know if I want to speak for these guys but what I'm hearing is, what I feel is that were not going to be ambiguous on this may end up residential because it's not going to end up residential.

TC – Our Comprehensive Plan is out there, the new one, its on our website, its been out there, it's something I would encourage you guys to look at, and see what the plan is, and the discussion that the community has had and all that, for this corner, and I understand that, the access thing, trust me, we've been dealing with that for 8 months, almost solid, trying to get access to a lot of those properties and that stuff over there, so I get it. But, I also understand the nature of our citizens and what we've had to do for the last 20 years I've been here. I have to go 7 miles any direction to get to a service or amenity, so I don't think anyone is going to have an issue of taking a right on Short Road to get in there to do something, or go back down to Palmer to come back out.

KN – We have to realize that when other options come along, which they will.

TC- They will come along

KN- That will change

TC- Maybe, I don't know, it's a possibility, I mean, its..., we want commercial, no doubt about it, its like Mr. Neilson has been saying, we need something there, it's got to happen, so, so there's no way we're gonna allow it to turn into residential...

MT- I hear what your saying, I don' think I've ever been in a position where its gone totally backwards and where I've got somebody saying more intensity, more intensity, which is a fun place to be, um, but if you remember, you know the property is currently zoned for a mix of R-2, you know, smorgasbord, we've got the entire property that the DA would apply to, and right now what's in front of you is this preliminary plat that does not include that mixed use area so we will have to have another application come in to do something with that mixed use area so I think that that discussion would be appropriate at that time, looking at whatever use gets proposed in that area, so you will have another bite at the apple, if..

TC- I get it but the red flag shows up when you say your not going to guarantee that it won't go residential and that's the red flag that comes up for us



MT-Sure

TC- You see what I mean

MT- Sure

TC- That was your words not mine

MT – I just want to be open with you guys. I hope you appreciate that, I'm not trying to shine you on, here but what I know is we've got an incredibly nice project here in front of you tonight, maybe one of the nicest here in the valley, were excited about it, we want to be part of Star and we think this is a good location to do it, and we heard what Staff said and were hearing exactly what you're saying and wanting to see a mix of uses, and that's what I'm seeing, were gonna have to be back here again for this piece here.

KN – I just want to say that personally I'll say that I lost sight of that so that's a good point

MT – Yeah, What were really asking tonight is this is what the DA, CUP and Preliminary Plat includes this area, and not this area or the part on the south side of the river, so same would go on that south side.



SCALE IN FEET

HWY 44

S. SHORT RD

W. MOON VALLEY RD

HWY 16

PROJECT  
LOCATION

MOON VALLEY  
SUBDIVISION NO.1

BOISE RIVER

**REUSE OF DRAWINGS**

J-U-B SHALL RETAIN ALL COMMON LAW, STATUTORY, COPYRIGHT AND OTHER RESERVED RIGHTS OF THESE DRAWINGS, AND THE SAME SHALL NOT BE REUSED WITHOUT J-U-B'S PRIOR WRITTEN CONSENT. ANY REUSE WITHOUT WRITTEN CONSENT BY J-U-B WILL BE AT CLIENT'S SOLE RISK AND WITHOUT LIABILITY OR LEGAL EXPOSURE TO J-U-B.

FILE: MV TOWNHOUSE VICINITY

JUB PROJ. #: 10-19-020

DRAWN BY: ###

DESIGN BY: ###

CHECKED BY: ###



J-U-B ENGINEERS, INC.

MOON VALLEY TOWNHOMES  
STAR, IDAHO

VICINITY MAP

SHEET

1

| NO | REVISION DESCRIPTION | BY | DATE | LAST UPDATED: 4/15/2021 |
|----|----------------------|----|------|-------------------------|
|    |                      |    |      |                         |
|    |                      |    |      |                         |
|    |                      |    |      |                         |



**J-U-B ENGINEERS, INC.**

J-U-B COMPANIES



**THE  
LANGDON  
GROUP**



**GATEWAY  
MAPPING  
INC.**

April 20, 2021

City of Star  
P.O. Box 130  
Star, Idaho 83669

**RE: MOON VALLEY TOWNHOMES SUBDIVISION– PRELIMINARY  
PLAT AND DA MODIFICATION APPLICATIONS**

To Whom It May Concern:

On behalf of our client, M3 Companies, please accept this request for a Preliminary Plat and Development Agreement Modification for Moon Valley Townhomes Subdivision; the proposed development will be constructed on an existing lot inside the recently approved Moon Valley Commons Subdivision in Star, Idaho. The proposed development includes a total of 156 townhome lots on 10.4 acres with a density of approximately 13 dwelling units per acre. The property is currently located in the City of Star and zoned M-U (Mixed Use.)



**Proposed Elevations for Moon Valley Townhomes**

## **Preliminary Plat**

The design of the project is intended to complement the traditional single-family homes in Moon Valley Subdivision and to provide upscale townhomes for Star residents. The 10.4-acre property will be divided into 156 residential lots and 1 common lot. All townhomes will be located on individual residential lots that would potentially allow for home ownership; the townhomes are classified in Star City Code as single family attached dwellings. The average lot size in the subdivision is 1,246 square feet in size. 36.5% of the subdivision property will be dedicated to qualified open space.

Moon Valley Townhomes has been designed for active empty nesters and young professionals; homes will range in size from 1,200 SF to 1,350 SF with a mix of 2 and 3-bedroom units. While the townhomes feature a unique design that is ideal for active residents interested in minimal upkeep of a yard and landscaping, the proposed townhomes include many features that are typical for traditional single-family homes; all units will include an attached 2-car garage and private outdoor courtyard. Residents will also have access to first class amenities including a clubhouse, resort-style pool, a dog park, and walking trails.

With high-pitched roofs, modern design, and upscale building materials, the architecture of the proposed Moon Valley Townhomes will complement the high-end homes in Moon Valley Subdivision. Attention to architectural detail has been given to create curb appeal and interest from all angles.

There are adequate public services available to this area to serve the subdivision; water and sewer will be extended from the north through Moon Valley Subdivision. The development will be served with public sewer and water by Star Sewer and Water District. Fire protection will be available through the Star Fire Department; M3 has met with the Star Fire Department to review the layout of the townhome development. Storm water will be retained on site and designed by a civil engineer in accordance with City of Star requirements.

Access to the development will be provided from the north through Moon Valley Subdivision. Internal access to residential lots will be provided via private roads. The private roads have been designed in accordance with Star Fire standards for private roads.



### **Development Agreement Modification**

A Development Agreement Modification is being requested to allow for attached single family homes to be constructed in the Mixed-Use Zoning District.

The development has been designed in accordance with the City of Star's Code and Comprehensive Plan. Please contact me at 376-7330 if you have any questions regarding this application.

Sincerely,  
**J-U-B ENGINEERS, Inc.**

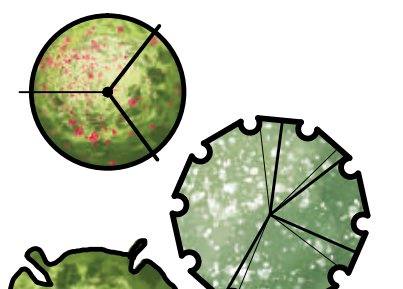


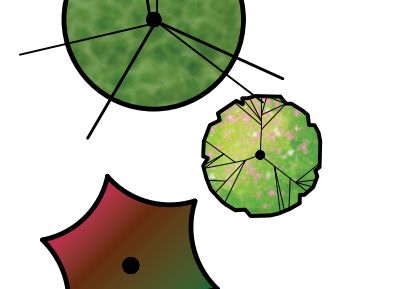

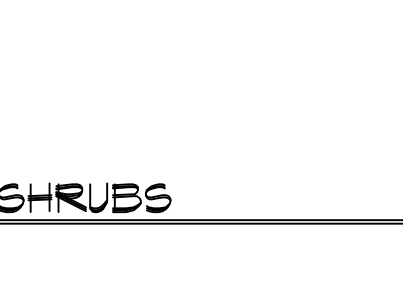
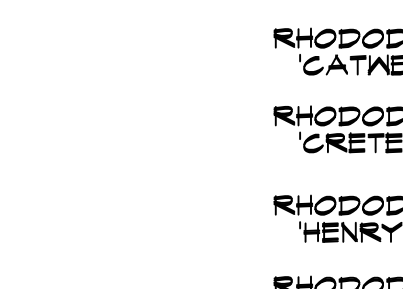


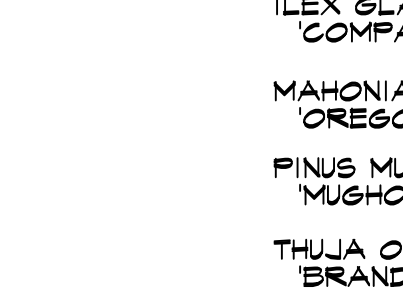
  
Wendy Shrief, AICP



PLANT LIST

SYMBOL QTY. BOTANICAL NAME  
COMMON NAME SIZE  
NOTES

TREES

|   |   |                                 |
|---|---|---------------------------------|
|                              | LIQUIDAMBAR STYRACIFLUA<br>"SWEETGUM"         | 24" BOX<br>SINGLE TRK.<br>STND. |
|                              | MAGNOLIA GRANDIFLORA<br>"SOUTHERN MAGNOLIA"   | 24" BOX<br>SINGLE TRK.<br>STND. |
|                              | GLIEDITSIA TRIACANTHOS<br>"HONEY LOCUST"      | 24" BOX<br>SINGLE TRK.<br>STND. |
|                              | PINUS STROBUS<br>"WHITE PINE"                 | 24" BOX<br>SINGLE TRK.<br>STND. |
|                              | KOELREUTERIA PANICULATA<br>"GOLDEN RAIN TREE" | 24" BOX<br>SINGLE TRK.<br>STND. |
|                              | PIRUS CALLERYANA<br>"BRADFORD PEAR"           | 24" BOX<br>SINGLE TRK.<br>STND. |
|                              | QUERCUS ROBUR X ALBA<br>"ENGLISH OAK"         | 24" BOX<br>SINGLE TRK.<br>STND. |
|                             | CORNUS FLORIDA<br>"FLOWER DOGWOOD"            | 24" BOX<br>SINGLE TRK.<br>STND. |
|                            | PRUNUS CERADIFERA<br>"CHERRY PLUM"            | 24" BOX<br>SINGLE TRK.<br>STND. |
|                            | PINUS FLEXILIS<br>"LIMBER PINE"               | 24" BOX<br>SINGLE TRK.<br>STND. |
| TREES WITHIN EXISTING OR FUTURE ROWS OR WITHIN 5 FEET OF PUBLIC<br>SIDEWALKS WILL INCLUDE DEEP ROOT BARRIERS. |   |                                 |

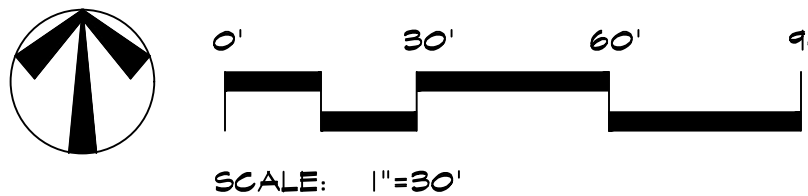
SHRUBS

|  |         |
|--|---------|
| RHODODENDRON<br>"CATALANSE ALBA"           | 15 GAL. |
| RHODODENDRON<br>"CRETE"                    | 15 GAL. |
| RHODODENDRON<br>"HENRY'S RED"              | 5 GAL.  |
| RHODODENDRON<br>"ENGLISH ROSEUM"           | 5 GAL.  |
| RHODODENDRON<br>"NORTHERN STARBURST"       | 5 GAL.  |
| KALMIA LATIFOLIA<br>"MOUNTAIN LAUREL"      | 5 GAL.  |
| BUXUS KOREAN<br>"BOXWOOD"                  | 5 GAL.  |
| DAFNE BURKWOODII<br>"THUNDER CLOUD"        | 5 GAL.  |
| JUNIFERUS FRITZERIANA                      | 5 GAL.  |
| JUNIFERUS CHINENSIS<br>"SEA GREEN"         | 5 GAL.  |
| ILEX GLABRA<br>"COMPACTA"                  | 5 GAL.  |
| MAHONIA AQUIFOLIUM<br>"OREGON GRAPE"       | 5 GAL.  |
| PIRUS MUGO<br>"MUGHO PINE"                 | 5 GAL.  |
| THUJA OCCIDENTALIS<br>"BRANDON"            | 5 GAL.  |
| WISTERIA FLORIBUNDA<br>"JAPANESE WISTERIA" | 5 GAL.  |
| VINCA MINOR<br>"DAARF PERIWINKLE"          | 5 GAL.  |

GROUND COVER & VINES

|  |        |
|--|--------|
| DAFNE BURKWOODII<br>"THUNDER CLOUD"            | 5 GAL. |
| COTONEASTER HORIZONTALIS<br>"ROCK COTONEASTER" | 5 GAL. |
| WISTERIA FLORIBUNDA<br>"JAPANESE WISTERIA"     | 5 GAL. |

PRELIMINARY LANDSCAPE PLAN  
MOON VALLEY TOWNHOMES  
CITY OF STAR, IDAHO



**HERMANN DESIGN GROUP**  
77-899 WOLF RD.  
SUITE 102  
PALM DESERT, CA 92211  
IDAHO LIC# XXX  
PH. (760) 777-9131  
FAX (760) 777-9132

LP-1



## CITY OF STAR, IDAHO





PRELIMINARY PLAT FOR  
MOON VALLEY TOWNHOMES SUBDIVISION

SITUATE IN THE NORTHEAST QUARTER OF SECTION 16,  
TOWNSHIP 4 NORTH, RANGE 1 WEST, BOISE MERIDIAN,

STAR, IDAHO  
2021

SHEET INDEX

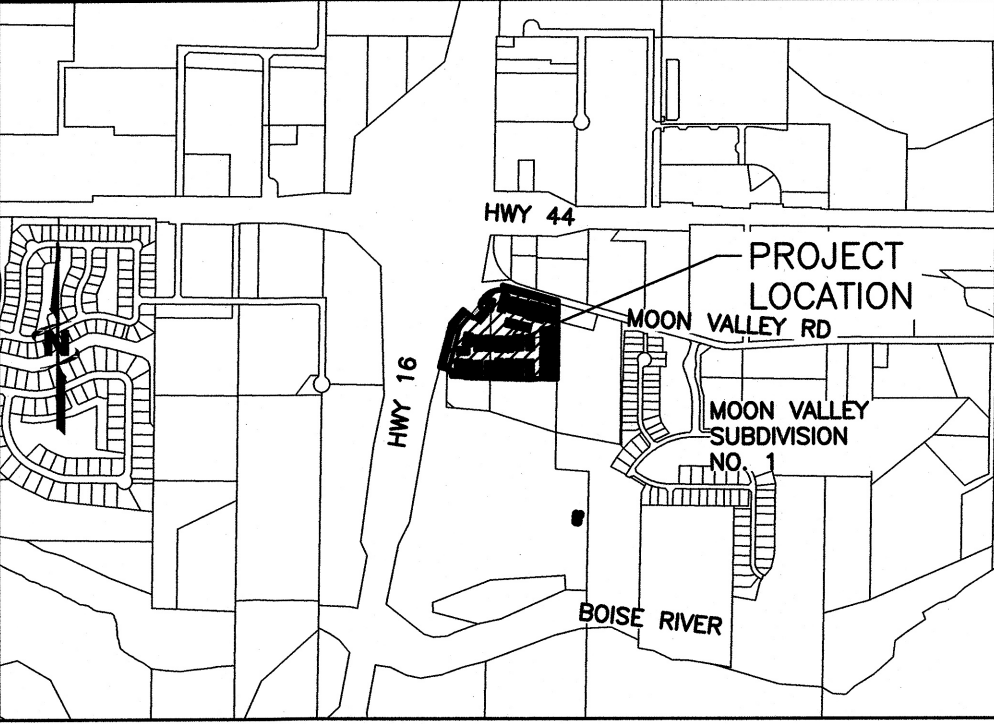
| SHEET NUMBER | SHEET TITLE                      |
|--------------|----------------------------------|
| PP-01        | BOUNDARY AND PROJECT INFORMATION |
| PP-02        | SITE DIMENSION PLAN              |
| PP-03        | EXISTING AND PROPOSED CONDITIONS |

BOUNDARY LEGEND

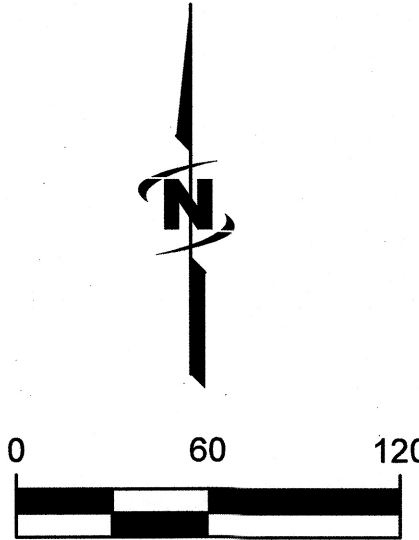
- Subdivision Boundary Line
- Section Line
- Center Line
- Right-of-way Line
- Existing Parcel Line
- Section Corner
- Quarter-Section Corner
- Property Corner
- Calculated Point

NOTES:

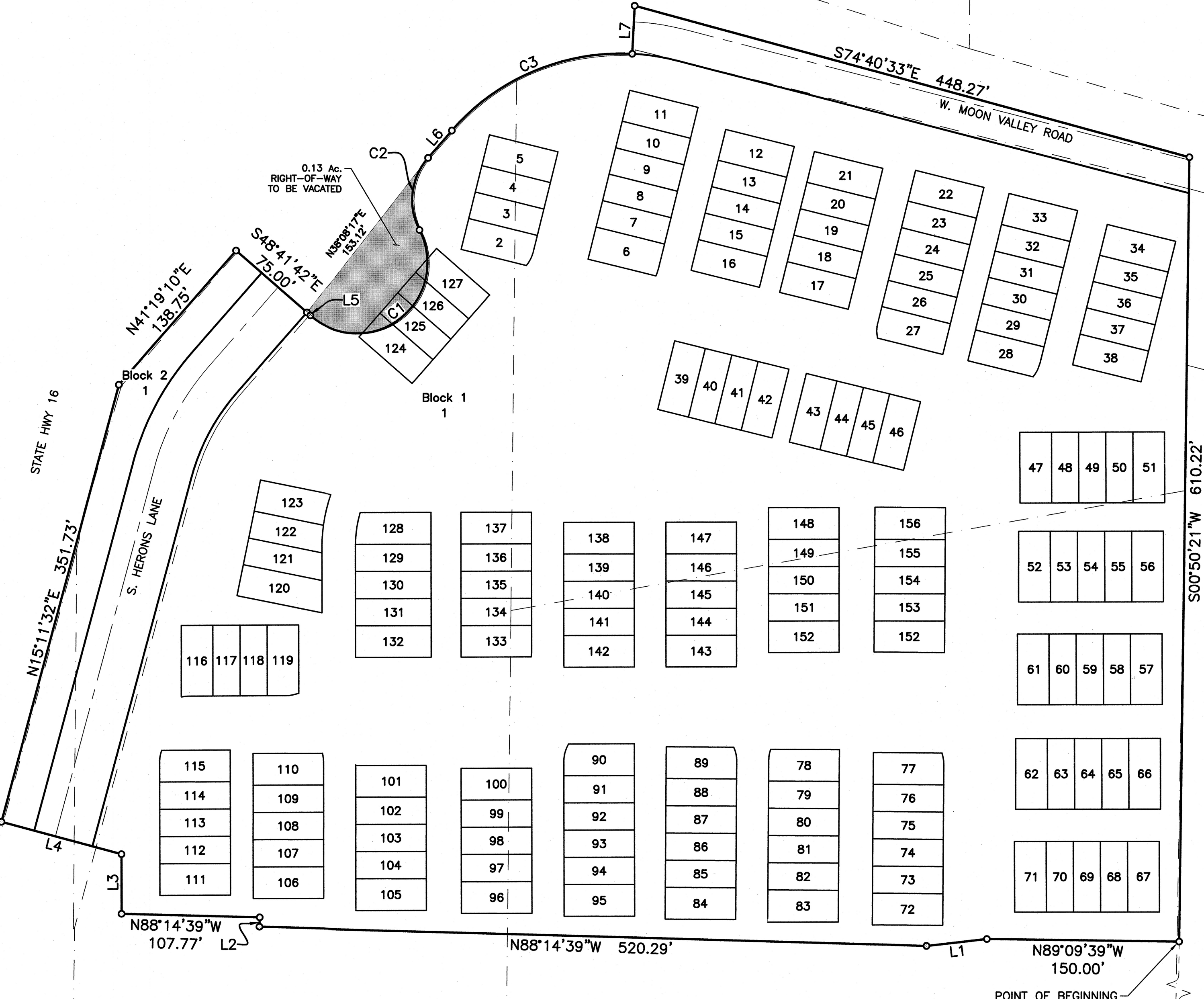
- CONTOUR AND SPOT ELEVATION DATA IS REFERENCED TO NAVD '88 DATUM.
- ALL STREETS PROPOSED IN THIS DEVELOPMENT ARE PUBLIC STREETS AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH CANYON HIGHWAY DISTRICT NO. 4 STANDARDS FOR PUBLIC STREETS WITH A MINIMUM GRADIENT OF 0.40%.
- ALL LOTS ARE RESIDENTIAL BUILDING LOTS, EXCEPT LOT 1, BLOCK 1, AND LOT 1, BLOCK 2 WHICH IS A COMMON AREA DRAINAGE STORAGE AND PARKING LOT, AND LOT 1, BLOCK 2 WHICH IS A COMMON AREA DRAINAGE STORAGE LOT, AND ARE OWNED AND MAINTAINED BY THE HOMEOWNERS ASSOCIATION.
- STORMWATER RUN-OFF GENERATED ON THIS SITE SHALL HAVE A QUALITY CONTROL TREATMENT PRIOR TO BEING CONVEYED TO ONSITE STORAGE FACILITIES.
- PUBLIC UTILITIES SHALL INCLUDE WATER, SEWER, ELECTRIC POWER, NATURAL GAS, TELEPHONE, AND CABLE TELEVISION.
- THIS SUBDIVISION WILL RECEIVE PRESSURIZED IRRIGATION FROM A PRESSURE IRRIGATION SYSTEM OWNED AND MAINTAINED BY THE HOME OWNERS ASSOCIATION.
- THIS DEVELOPMENT RECOGNIZES SECTION 22-4503 OF IDAHO CODE, RIGHT TO FARM ACT, WHICH STATES, "NO AGRICULTURAL OPERATION, AGRICULTURAL FACILITY OR EXPANSION THEREOF SHALL BE OR BECOME A NUISANCE, PRIVATE OR PUBLIC, BY ANY CHANGED CONDITIONS IN OR ABOUT THE SURROUNDING NONAGRICULTURAL ACTIVITIES AFTER IT HAS BEEN IN OPERATION FOR MORE THAN ONE (1) YEAR, WHEN THE OPERATION, FACILITY OR EXPANSION WAS NOT A NUISANCE AT THE TIME IT BEGAN OR WAS CONSTRUCTED. THE PROVISIONS OF THIS SECTION SHALL NOT APPLY WHEN A NUISANCE RESULTS FROM THE IMPROPER OR NEGLIGENT OPERATION OF AN AGRICULTURAL OPERATION, AGRICULTURAL FACILITY OR EXPANSION THEREOF."
- DOMESTIC AND FIRE PROTECTION WATER SHALL BE PROVIDED BY THE STAR SEWER AND WATER DISTRICT.
- SANITARY SEWER COLLECTION SHALL BE PROVIDED BY THE STAR SEWER AND WATER DISTRICT.
- BUILDING SETBACKS AND DIMENSIONAL STANDARDS SHALL BE IN ACCORDANCE WITH THE CITY OF STAR STANDARDS IN EFFECT AT THE TIME OF DEVELOPMENT OR WITH THE APPROVED DEVELOPMENT AGREEMENT.
- ANY RE-SUBDIVISION OF THIS PLAT SHALL COMPLY WITH THE APPLICABLE ZONING REGULATIONS IN EFFECT AT THE TIME OF THE RE-SUBDIVISION.
- THE OWNER SHALL COMPLY WITH IDAHO CODE, SECTION 31-3805 OR ITS PROVISIONS THAT MAY APPLY TO IRRIGATION RIGHTS.
- A STORMWATER DRAINAGE EASEMENT SHALL BE RESERVED ON THE DRAINAGE LOTS. LANDSCAPING OVER SAID LOTS SHALL BE MAINTAINED BY THE HOMEOWNERS ASSOCIATION AND SHALL PROVIDE HEAVY MAINTENANCE OF THE STORM DRAINAGE FACILITIES AS DEFINED IN THE COVENANTS, CONDITIONS AND RESTRICTIONS FOR MOON VALLEY TOWNHOMES SUBDIVISION.
- PORTIONS OF THIS PROPERTY ARE LOCATED IN ZONE AE OF THE FLOOD INSURANCE RATE MAPS NO. 16001C0130 J, COMMUNITY PANEL NO. 0130 J, WHICH BEARS AN EFFECTIVE DATE OF JUNE 19, 2020 AND ARE IN A SPECIAL FLOOD HAZARD AREA.
- ALL LOTS SHALL HAVE PERMANENT 0' WIDE SIDE LOT EASEMENTS, 3' WIDE FRONT LOT EASEMENTS AND A 5' WIDE REAR LOT EASEMENTS FOR PUBLIC UTILITIES, IRRIGATION, AND LOT DRAINAGE.
- EASEMENTS FOR SEWER/WATER FACILITIES WILL BE REQUIRED WHERE PLACED OUTSIDE OF PUBLIC RIGHT OF WAY. LOT 1, BLOCK 1 SHALL HAVE EASEMENTS FOR THE STAR SEWER AND WATER DISTRICT UTILITIES.



VICINITY MAP  
SCALE: 1"=1500'

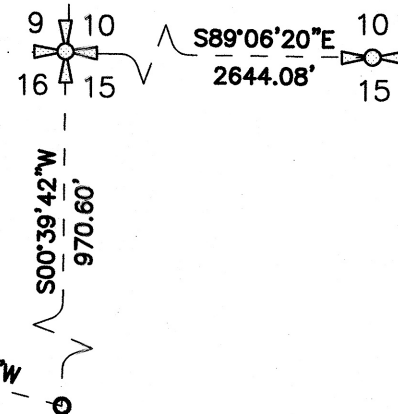


SCALE IN FEET



| Line Table |             |        |
|------------|-------------|--------|
| Line #     | Direction   | Length |
| L1         | S83°31'12"W | 47.39' |
| L2         | N01°45'21"E | 7.31'  |
| L3         | N00°01'18"E | 46.47' |
| L4         | N74°48'28"W | 96.89' |
| L5         | S48°40'50"E | 2.00'  |
| L6         | N41°19'10"E | 28.18' |
| L7         | N02°59'37"E | 37.46' |

BASIS OF BEARINGS



| Curve Table |         |         |            |               |
|-------------|---------|---------|------------|---------------|
| Curve #     | Length  | Radius  | Delta      | Chord Bearing |
| C1          | 152.42' | 55.00'  | 158°46'56" | N51°55'44"E   |
| C2          | 60.02'  | 50.00'  | 68°46'42"  | S06°55'37"W   |
| C3          | 157.85' | 175.00' | 51°40'52"  | S67°09'38"W   |

| EXISTING |                         | PROPOSED |                           |
|----------|-------------------------|----------|---------------------------|
| ---      | SANITARY SEWER LINE     | ---      | WATER LINE                |
| ---      | GRAVITY IRRIGATION PIPE | ---      | PRESSURE IRRIGATION LINE  |
| ---      | STORM DRAIN LINE        | ---      | CENTERLINE                |
| ---      | LOT LINE                | ---      | 6" VERTICAL CURB & GUTTER |
| ---      | ROLLED CURB & GUTTER    | ---      | 5' SIDEWALK               |
| ○        | SEWER MANHOLE           | ○        | IRRIGATION MANHOLE        |
| ---      | DITCH FLOW LINE         | ---      | STREET LIGHT              |
| ---      | TREE DECIDUOUS          | ---      | TREE CONIFER              |
| ---      | EDGE OF PAVEMENT        | ---      | GAS LINE                  |
| ---      | FENCE LINE              | ---      | FIRE HYDRANT              |
| ---      | 5' CONTOUR LINE         | ---      | 1' CONTOUR LINE           |
| ---      | DRAINAGE FACILITY       | ---      |                           |

IRRIGATION DISTRICT  
CANYON COUNTY WATER CO. LTD  
PO BOX 11  
STAR, ID 83669

LAND SURVEYOR  
ROBERT KAZARINOFF, P.L.S.  
J-U-B ENGINEERS  
2760 W. EXCURSION LN. STE. 400  
MERIDIAN, ID 83642  
208-376-7330

CIVIL ENGINEER  
W. SCOTT WONDERS, P.E.  
J-U-B ENGINEERS  
2760 W. EXCURSION LN. STE. 400  
MERIDIAN, ID 83642  
208-376-7330

OWNER  
BALL REAL ESTATE INVESTMENTS INC  
7033 E. GREENWAY PKWY  
STE 300  
SCOTTSDALE, AZ 85254

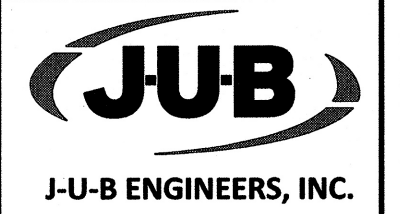
DEVELOPER  
M3 COMPANIES  
1087 W. RIVER STREET  
SUITE 310  
BOISE, ID 83702  
MARK TATE 208-939-6263

LAND USE SUMMARY

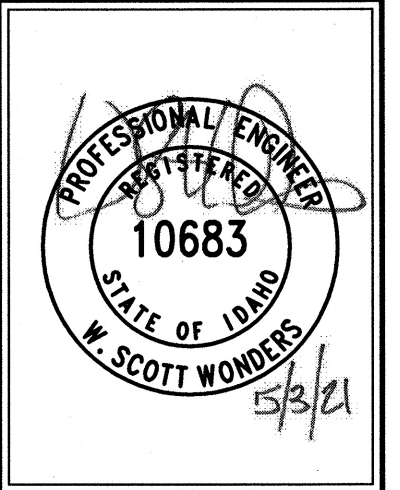
|                      |                  |
|----------------------|------------------|
| BOUNDARY AREA:       | 12.06 AC.        |
| VACATED ROW AREA:    | 0.13 AC.         |
| TOTAL AREA:          | 12.19 AC.        |
| TOTAL LOTS:          | 157 LOTS         |
| RESIDENTIAL LOTS:    | 156 LOTS         |
| COMMON AREA LOTS:    | 1 LOT            |
| RESIDENTIAL DENSITY: | 12.80 DU/AC      |
| OPEN SPACE:          | 4.26 AC. (34.9%) |
| AVERAGE LOT SIZE:    | 1,246 S.F.       |
| SMALLEST LOT:        | 1,164 S.F.       |
| EXISTING ZONE:       | MU               |



BOUNDARY  
CERTIFICATION



J-U-B ENGINEERS, INC.  
2760 W. EXCURSION LN.  
Suite 400  
Meridian, ID 83642  
Phone: 208.376.7330  
www.jub.com



| NO. | REVISION | DESCRIPTION | DATE |
|-----|----------|-------------|------|
|     |          |             |      |
|     |          |             |      |
|     |          |             |      |
|     |          |             |      |
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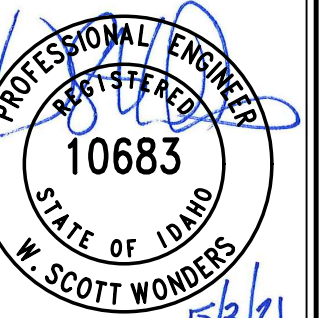
MOON VALLEY TOWNHOMES SUBDIVISION  
STAR, IDAHO

BOUNDARY AND PROJECT INFORMATION




2760 W. Excursion Ln.  
Suite 400  
Meridian, ID 83642

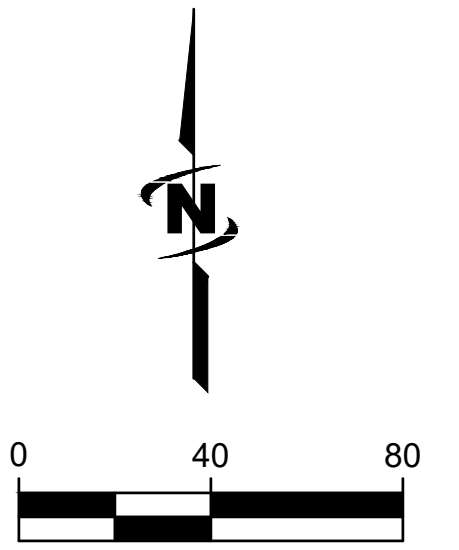
Phone: 208.376.7330  
[www.jub.com](http://www.jub.com)

[illegible]

STAR, IDAHO

|   |
|---|
| 10-21-039_PRE-PLAT  |
| PROJ. #: 10-21-039  |
| DESIGN BY: ----   |
| CHECKED BY: ----  |
| <br>AT FULL SIZE, IF NOT ONE INCH, SCALE ACCORDINGLY |
| DATE: 5/3/2021  |
| SHEET NUMBER:   |

PP-02



SCALE IN FEET

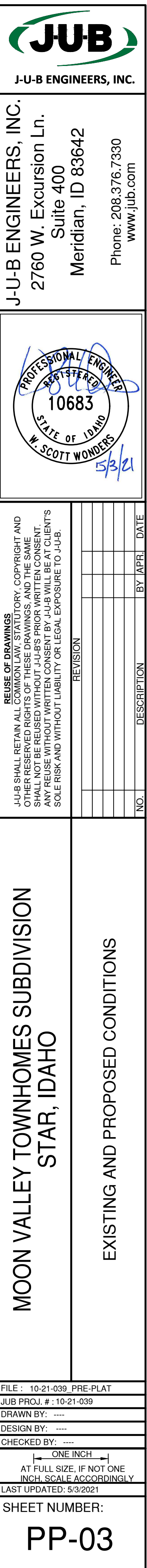
## PARKING SUMMARY

|                              |            |
|------------------------------|------------|
| RESIDENTIAL UNITS:           | 156 UNITS  |
| REQUIRED PARKING:            | 351 SPACES |
| REQUIRED ACCESSIBLE PARKING: | 8 SPACES   |
| TOTAL REQUIRED PARKING:      | 358 SPACES |
| PROVIDED GARAGE PARKING:     | 310 SPACES |
| PROVIDED GUEST PARKING:      | 55 SPACES  |
| PROVIDED ACCESSIBLE PARKING: | 8 SPACES   |
| PROVIDED TOTAL PARKING:      | 373 SPACES |



Plot Date: 5/4/2021 7:00 AM Plotted By: Everett Earnest  
Date Created: 5/3/2021 \\JUB.COM\CENTRAL\CLIENTS\IDM3\PROJECTS\10-21-039 MOON VALLEY TOWNHOMES\PRE-PLAT\CAD\SHEET\10-21-039 PRE-PLAT.DWG









**Your Safety • Your Mobility  
Your Economic Opportunity**

**Staff Technical Report  
6/16/2021**

---

**Development:** Moon Valley Townhomes

**Lead Agency:** City of Star  
Shawn Nickel  
[Snickel@staridaho.org](mailto:Snickel@staridaho.org)  
208-286-7247

**Location:** Southeast corner of SH-16 / SH-44  
SH-44 MP 13.2

**Applicant:** M3 Companies  
Mark Tate  
1087 W. River Street, Suite 310  
Boise, ID 83702  
[MTate@m3companiesllc.com](mailto:MTate@m3companiesllc.com)  
Phone: 208.939.6263

**Consultant:** Kittelson & Associates  
John Ringert  
CR Engineering, Inc.  
101 S. Capitol Blvd, Suite 600  
[JRINGERT@kittelson.com](mailto:JRINGERT@kittelson.com)  
Phone: 208-338-2683

**Staff Contact:** Erika Bowen  
Idaho Transportation Department  
District 3 Development Services Technical Engineer  
[Erika.Bowen@itd.idaho.gov](mailto:Erika.Bowen@itd.idaho.gov)  
Phone: 208-265-4312 ext. #7

## Proportionate Share Contribution

ITD District 3 has issued *Memo 39-Development Proportionate Share Contribution (Updated 11-13-2020)* as a means to request equitable contribution from developers to improve public facilities needed to serve new growth and development.

ITD does not have jurisdictional authority to require proportionate share contribution from the developer because they are not asking for a direct access approach. ITD and the city of Star have entered into an Intergovernmental Agreement for the city to collect proportionate share contributions on the department's behalf to be used towards future ITIP projects on the State highway system jointly selected by the two agencies.

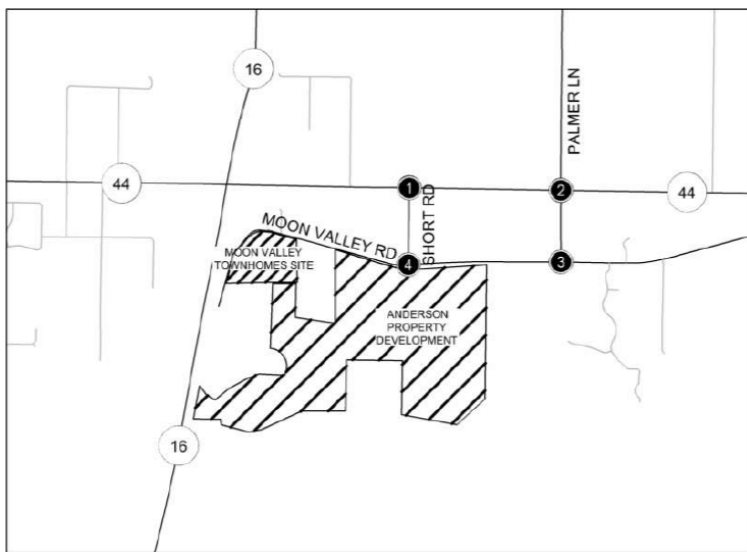
| Intersection                                 | Proportionate Share Contribution |
|--|----------------------------------|
| SH-44 / Short Road                           | \$52,039                         |
| SH-44 / Palmer Lane                          | \$14,350                         |
| SH-16 / SH-44                                | N/A                              |
| <b>TOTAL</b>                                 | <b>\$66,389</b>                  |
| <b><i>Approximate per household unit</i></b> | <b><i>\$426</i></b>              |

## Traffic Impact Study Overview

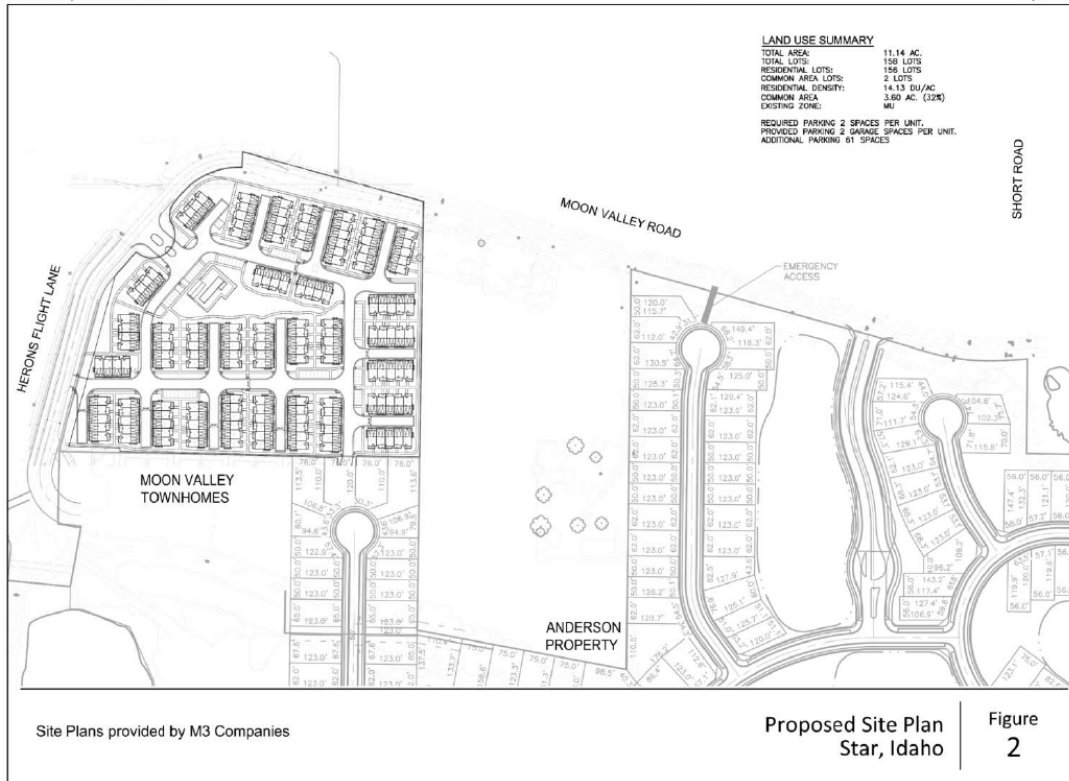
### 1. Proposed Development

The proposed Moon Valley Townhomes development is estimated to contain 156 low-rise multi-family townhomes. Anticipated buildout year is 2023.

### 2. Vicinity Map







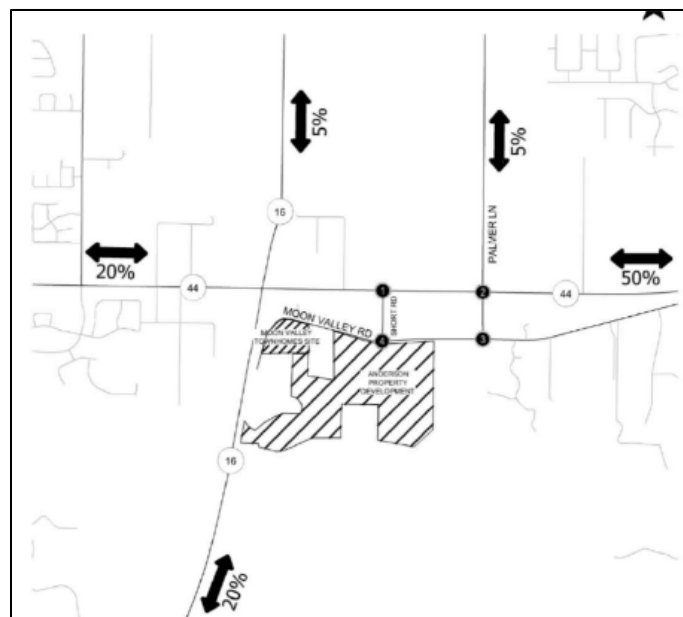
### 3. Trip Generation:

**Table 12. Proposed Trip Generation**

| Land Use                       | ITE Code | Units <sup>1</sup> | Daily | Weekday AM Peak Hour |    |     | Weekday PM Peak Hour |    |     |
|--------------------------------|----------|--------------------|-------|----------------------|----|-----|----------------------|----|-----|
|                                |          |                    |       | Total                | In | Out | Total                | In | Out |
| Multifamily Housing (Low-Rise) | 220      | 156                | 1,139 | 73                   | 17 | 56  | 88                   | 55 | 33  |

<sup>1</sup> Dwelling Units

### 4. Trip Distribution



## 5. Traffic Impact Study Recommendations

*Dated: April 2021*

Kittelsohn & Associates prepared a traffic impact analysis for the proposed Moon Valley Townhomes Development. Below is an executive summary of the findings as presented by Kittelsohn & Associates. The following executive summary is not the opinion of ITD staff. ITD staff has reviewed the submitted traffic impact study for consistency with ITD policies and practices and may have additional and/or varied requirements beyond what is noted in the summary.

### Existing Conditions

- The study evaluated four off-site intersections during the weekday a.m. and weekday p.m. peak period of a typical weekday (Tuesday – Thursday).
- All study intersections were found to operate at acceptable operating standards during the existing weekday a.m. and weekday p.m. peak hours with the following exception:
  - SH 44/Palmer Lane: During the weekday p.m. peak hour, the critical northbound movement is anticipated to operate at LOS F, which falls below ACHD policy of LOS E, as well as ITD policy of LOS D. But the northbound movement remains well under capacity with a v/c ratio of 0.50.
  - A planning-level evaluation of MUTCD volume-based signal warrants showed the 8-hour, 4-hour, and peak hour signal warrants are met. However, ITD has a signal programmed in the ITIP for 2027.
- All ACHD study roadway segments operate at an acceptable level of service.
- Crash data at the study intersection for the most recent five years (2015-2019) was analyzed for any existing crash trends. No crash trends were found.
- Site observation revealed the following:
  - There is currently no stop sign for the southbound approach on South Short Road at the Moon Valley Road intersection, which is not consistent with the Palmer Lane intersection which has a stop sign. Installation of a new stop sign should be considered by ACHD.
  - There is a tight curve on Moon Valley Road that limits intersection and stopping sight distance. The curve does not appear to be designed to accommodate the posted speed of 35 mph.

### Year 2023 Background Traffic Conditions

- Year 2023 background traffic volumes were forecast using a 4 percent annual growth rate and included the addition of in-process traffic from the Anderson Property, Snoqualmie Subdivision, and Amazon Falls North developments.
- Operations were evaluated with the planned widening of SH 44 to a five-lane cross-section identified in ITD's Statewide Transportation Improvement Plan (STIP) for construction in 2023.
- Year 2023 background traffic analysis (without inclusion of site-generated traffic) found that all the study intersections are expected to continue to operate at acceptable operating standards during the weekday a.m. and p.m. peak hours with the following exceptions:

- SH 44/Short Road
  - The northbound left-turn movement is forecast to operate at LOS E during the weekday a.m. peak hour. This meets ACHD policy but not ITD policy.
  - Due to a signal being planned at the SH 44/Palmer Road intersection and its proximity to the SH16/SH44 intersection and planned future interchange ramps, signalization of this intersection would not meet ACHD or ITD signal spacing criteria.
- SH 44/Palmer Lane
  - The southbound movement is forecast to operate at LOS E during the weekday p.m. peak hour.
  - A planning-level evaluation of MUTCD volume-based signal warrants identified that the 8-hour volume, 4-hour volume, and peak hour warrants are met under year 2023 background traffic volumes.
    - With the planned widening of SH 44 and a traffic signal, the intersection is forecast to operate at LOS B in the weekday p.m. peak hour.
- All ACHD study roadway segments continue to operate at an acceptable level of service.
- The daily traffic projected on Short Road south of SH 44 is projected to exceed the ACHD threshold for a local street of 2,000 daily trips.

#### Year 2023 Total Traffic Conditions

- The year 2023 total traffic analysis (with inclusion of site-generated traffic) found that all the study intersections are expected to continue to operate at acceptably during the weekday a.m. and p.m. peak hours with the same exceptions as under background traffic conditions:
  - SH 44/Short Road
    - The northbound left-turn movement is forecast to operate at LOS E during the weekday a.m. and weekday p.m. peak hours. The critical movements are anticipated to operate below capacity during both analysis periods.
    - Signalization of this intersection would not meet ACHD or ITD signal spacing criteria.
  - SH 44/Palmer Lane
    - The southbound movement is still forecast to operate at LOS E during the weekday p.m. peak hour but remains under capacity.
    - Per the analysis in the 2023 background conditions, traffic signal warrants are met.
      - With the planned widening of SH 44, a traffic signal is forecast to operate below capacity at LOS B during both the weekday a.m. and weekday p.m. peak hours.
- The site access operates acceptably during the weekday a.m. and p.m. peak hours.
- All ACHD study roadway segments continue to operate at an acceptable level of service.

## Turn Lane Warrant Evaluation

- With the low volumes on West Moon Valley Road and the relatively low number of trips generated by the site, no turn lanes are warranted at the proposed site access intersection.
- The only off-site intersection identified as warranting a turn lane for a movement impacted by site traffic is the SH 44/Short Road intersection.
  - An eastbound right-turn lane on SH 44 was found to be warranted under existing traffic volumes.

## RECOMMENDATIONS

Based on the analyses and findings, recommendations were developed accordingly. The following mitigations are recommended with buildout of the Moon Valley Townhomes' development.

### Existing Conditions

- Install a stop sign on the southbound approach at the Moon Valley Road/Short Road intersection.
- Consider installation of an eastbound right turn lane at the SH 44/Short Road intersection based on turn lane warrants. The right-turn volume is warranted under existing volumes. This was also identified in the Anderson Property Development TIS. This improvement could also be done at the time ITD widens SH 44 to avoid constructing a new improvement that is removed or replaced in two years.
- Trim the existing vegetation on the south side of Moon Valley Road and east side of Herons Flight Lane to the west of Short Road to approximately 3 feet or less between the edge of pavement and the existing fence line.
- Conduct a speed study and curve evaluation to determine if reduction of the posted speed on Moon Valley Road to 20 mph on Moon Valley Road and Herons Flight Lane is warranted. If the speed can be reduced to 20 mph, reduce the speed limit on Moon Valley Road and Herons Flight Lane to 20 mph.

### Background Traffic Conditions

- Consider allowing the daily traffic on Short Road to exceed the 2,000 daily traffic policy threshold or re-classifying Short Road from a local road to a collector road.

### Total Traffic Conditions (With Site Development)

- Improve intersection sight distance at the site access through the following:
  - Per the recommendation under existing conditions, conduct a speed study and curve evaluation to determine if reduction of the posted speed on Moon Valley Road and Herons Flight Lane is warranted. If the speed can be reduced to 20 mph, reduce the speed limit on Moon Valley Road and Herons Flight Lane to 20 mph.
  - If the speed cannot be reduced, relocate the site approximately 160-180 feet south to a location in which adequate intersection sight distance can be obtained.
  - Additionally, the following measures should be taken to ensure adequate sight distance is maintained:

- Trim existing vegetation along Herons Flight Lane north of the proposed site access which currently limits intersection sight distance to under 3.5 feet in height.
- Future roadside landscaping and shrubbery should be maintained below site lines (3.5 feet).

## ITD Proportionate Share Contributions

### 1. SH-44 and Palmer Lane

*\*All supporting documentation in Appendix A*

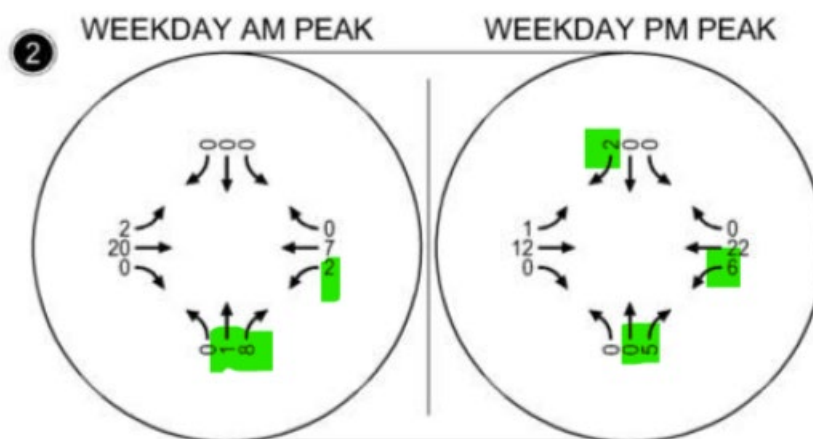
ITD programmed the Palmer Lane signalization project for 2027 construction with the understanding that the department would request proportionate share contributions from new nearby developments to help accelerate the project.

Staff estimates the design, construction and right-of-way costs as \$1,479,414.

Separately, ITD has a programmed project to widen SH-44 through the Palmer Lane intersection to the ultimate configuration.

ITD is currently working on the SH-44 corridor plan with a design year of 2045. The Traffic Analysis for this segment of the corridor has been reviewed by FHWA and deemed acceptable. Due to the nature of the signalization project and that its benefit and construction focuses on the local road traffic, proportionate share shall be based on Palmer Lane site trips versus Palmer Lane intersection trips at 2045.

#### Total Site Traffic





## Total 2045 Traffic

### 2045 Build

Table 5. 2045 Build intersection turning movement volumes – AM peak hour (7:00-8:00 AM)

| Int. No. | Intersection Name | Northbound |      |       | Southbound |      |       | Eastbound |       |       | Westbound |       |       |
|----------|-------------------|------------|------|-------|------------|------|-------|-----------|-------|-------|-----------|-------|-------|
|          |                   | Left       | Thru | Right | Left       | Thru | Right | Left      | Thru  | Right | Left      | Thru  | Right |
| 23       | Palmer Lane/SH-44 | 4          | 79   | 35    | 66         | 94   | 397   | 277       | 2,444 | 3     | 5         | 1,418 | 87    |

Table 6. 2045 Build intersection turning movement volumes – PM peak hour (4:45-5:45 PM)

| Int. No. | Intersection Name | Northbound |      |       | Southbound |      |       | Eastbound |       |       | Westbound |       |       |
|----------|-------------------|------------|------|-------|------------|------|-------|-----------|-------|-------|-----------|-------|-------|
|          |                   | Left       | Thru | Right | Left       | Thru | Right | Left      | Thru  | Right | Left      | Thru  | Right |
| 23       | Palmer Lane/SH-44 | 12         | 160  | 37    | 66         | 99   | 460   | 299       | 1,886 | 21    | 33        | 2,540 | 285   |

| Proportionate Share Calculations       |                  |              |
|--|------------------|--------------|
| AM Site = 11                           | AM Total = 1,047 | AM % = 1.05  |
| PM Site = 13                           | PM Total = 1,472 | PM % = 0.88  |
| Average Proportionate Share Percentage |                  | Avg % = 0.97 |
| SH-44 / Palmer Lane Cost Estimate      |                  | \$1,479,414  |
| Proportionate Share Contribution       |                  | \$14,350     |

**Staff calculates the developer's proportionate share to be \$14,350 (0.97%) based on Palmer Lane site trips versus Palmer Lane intersection trips at 2045 buildout.**

## 2. SH-44 and Short Road

\*All supporting documentation in Appendix B

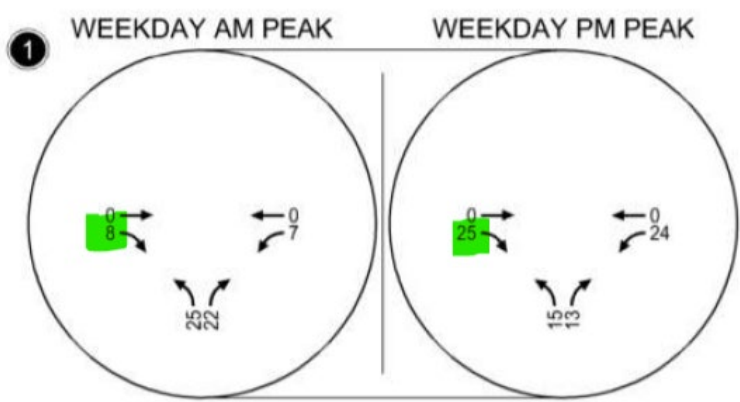
Per the TIS, the intersection of SH-44 and Short road operates at acceptable levels of operation with SH-44 being widened to a five lane section. ITD has a project programmed in 2023 to widen SH-44.

The TIS performed a right turn lane warrant for the intersection and found that an eastbound right turn lane is warranted under existing traffic conditions. The requirement to install a right turn lane was mentioned in ITD's development condition memo to the Anderson Property development adjacent to Moon Valley Townhomes. ITD did not have jurisdiction to require this of Anderson Property, but requested it from the city of Star.

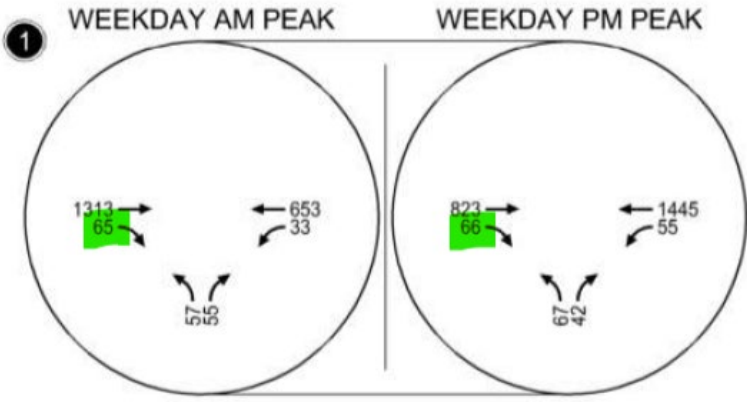
ITD staff estimates the design, construction and right-of-way costs to construct an eastbound right turn lane at Short Road as **\$1,479,414.**

Proportionate share shall be based on site trips utilizing the right turn lane versus total right turn movements at 2023 total buildout.

**Total Site Traffic**



**Total 2023 Traffic**



| Proportionate Share Calculations                 |               |               |
|--|---------------|---------------|
| AM Site = 8                                      | AM Total = 65 | AM % = 12.31  |
| PM Site = 25                                     | PM Total = 66 | PM % = 37.88  |
| Average Proportionate Share Percentage           |               | Avg % = 25.10 |
| SH-44 / Short Road Right Turn Lane Cost Estimate |               | \$207,325     |
| Proportionate Share Contribution                 |               | \$52,039      |

**Staff calculates the developer’s proportionate share to be \$52,039 (25.10%) based on right turn site trips versus total right turn trips at 2023 total buildout.**

### 3. SH-44 and SH-16

Analysis was not requested at the intersection of SH-16 / SH-44. ITD has been requesting all nearby developments to contribute to proportionate share at this intersection for unique trips. All of Moon Valley Townhomes sit traffic are entering the SH-44 / Short Road intersection or SH-44 / Palmer lane intersection. ITD is already requesting proportionate share contribution towards improvements at these two intersections. See above sections.

***Staff does not recommend a proportionate share contribution at the intersection of SH-16 / SH-44.***

*\*ITD Staff Recommendations are intended to assure that the proposed development will not place an undue burden on the existing State Highway system within the vicinity impacted by the proposed development.*

*\*\* Recommendations included in ITD's Staff Technical Report along with any development conditions (see associated Permit Committee Agenda/Minutes) is only valid for the period of one year from the date of the TIS report. ITD reserves the right to request an updated TIS to reflect current traffic conditions if an approved encroachment application and/or proportionate share contribution are not obtained/provided within one year.*

### 6. Appendices

|            |                                   |
|------------|-----------------------------------|
| Appendix A | SH-44 / Palmer Lane Documentation |
| Appendix B | SH-44 / Short Road Documentation  |

# Appendix A

## SH-44 / Palmer Lane Documentation

**Table 13. Year 2023 Total Intersection Operations – Weekday AM and PM Peak Hour**

| Intersection        | Intersection Control        | Intersection Delay (sec), AM / PM | Lane Group | AM Peak Hour     |                  |                    | PM Peak Hour     |                  |                    |
|---------------------|-----------------------------|-----------------------------------|------------|------------------|------------------|--------------------|------------------|------------------|--------------------|
|                     |                             |                                   |            | V/C <sup>1</sup> | LOS <sup>2</sup> | Delay <sup>3</sup> | V/C <sup>1</sup> | LOS <sup>2</sup> | Delay <sup>3</sup> |
| SH 44 / Palmer Lane | TWSC<br>5 Lanes on SH 44    | N/A                               | EBL        | 0.11             | A                | 9.7                | 0.13             | C                | 15.4               |
|                     |                             |                                   | EBTR       | -                | -                | -                  | -                | -                | -                  |
|                     |                             |                                   | WBL        | 0.01             | B                | 12.8               | 0.02             | A                | 10.0               |
|                     |                             |                                   | WBTR       | -                | -                | -                  | -                | -                | -                  |
|                     |                             |                                   | NBLTR      | 0.08             | C                | 18.4               | 0.18             | D                | 31.4               |
|                     |                             |                                   | SBLTR      | 0.37             | C                | 22.3               | 0.59             | <b>E</b>         | <b>48.5</b>        |
|                     | TWSC<br>Ex 3 Lanes on SH 44 | N/A                               | EBL        | 0.11             | A                | 9.7                | 0.13             | C                | 15.4               |
|                     |                             |                                   | EBTR       | -                | -                | -                  | -                | -                | -                  |
|                     |                             |                                   | WBL        | 0.01             | B                | 12.8               | 0.02             | A                | 10.0               |
|                     |                             |                                   | WBTR       | -                | -                | -                  | -                | -                | -                  |
|                     |                             |                                   | NBLTR      | 0.16             | D                | 33.6               | 0.12             | C                | 20.6               |
|                     |                             |                                   | SBLTR      | 0.69             | <b>F</b>         | <b>62.9</b>        | 0.89             | <b>F</b>         | <b>133.3</b>       |
|                     |                             |                                   | EBLT       | 0.02             | A                | 7.3                | 0.01             | A                | 7.3                |

### SH 44 (West State Street) / Palmer Lane

As highlighted in Table 13, the SH 44/Palmer Lane intersection does not meet ACHD and ITD operating policy for the southbound approach under year 2023 total traffic conditions during the weekday p.m. peak hour. This is due to the very high eastbound and westbound traffic volumes on SH 44, which result in very few gaps in traffic for left-turning and through movements on Palmer Lane. Per the 2023 background conditions analysis, this intersection meets traffic signal warrants and is in the ITD ITIP for signalization in 2027. Table 16 shows the operations under year 2023 total traffic conditions with the proposed buildout of SH 44/Palmer Lane intersection with a traffic signal.

**Table 16. Year 2023 Total Traffic Conditions Traffic Signal – SH 44/Palmer Lane**

| Intersection        | Intersection (AM / PM) |       |              | Lane Group | Weekday AM Peak Hour |     |              | Weekday PM Peak Hour |     |              |
|---------------------|------------------------|-------|--------------|------------|----------------------|-----|--------------|----------------------|-----|--------------|
|                     | V/C                    | LOS   | Delay (sec.) |            | V/C                  | LOS | Delay (sec.) | V/C                  | LOS | Delay (sec.) |
| SH 44 / Palmer Lane | 0.68 / 0.68            | B / B | 12.9 / 14.1  | EBL        | 0.20                 | A   | 6.8          | 0.21                 | B   | 11.4         |
|                     |                        |       |              | EBT        | 0.81                 | B   | 14.1         | 0.45                 | A   | 7.7          |
|                     |                        |       |              | EBR        | 0.81                 | B   | 14.0         | 0.45                 | A   | 7.6          |
|                     |                        |       |              | WBL        | 0.03                 | A   | 9.8          | 0.04                 | A   | 6.1          |
|                     |                        |       |              | WBT        | 0.48                 | B   | 10.4         | 0.84                 | B   | 16.5         |
|                     |                        |       |              | WBR        | 0.48                 | B   | 10.4         | 0.84                 | B   | 16.5         |
|                     |                        |       |              | NBL        | 0.00                 | A   | 0.0          | 0.07                 | C   | 26.4         |
|                     |                        |       |              | NBTR       | 0.16                 | C   | 22.4         | 0.11                 | C   | 27.6         |
|                     |                        |       |              | SBL        | 0.10                 | B   | 18.8         | 0.08                 | C   | 25.9         |
|                     |                        |       |              | SBTR       | 0.27                 | B   | 17.6         | 0.63                 | C   | 32.6         |

Similar to background conditions, a traffic signal is forecast to operate below capacity at LOS B for all movements during both the weekday a.m. and weekday p.m. peak hours with the planned widening of SH 44.

Therefore, upon such time that SH 44 is widened to a five-lane cross-section, converting the stop-controlled SH 44/Palmer Lane intersection to a traffic signal represents a viable improvement to help alleviate the forecast LOS F conditions experienced under year 2023 total traffic conditions.

Site traffic is projected to be approximately 1.9 percent and 2.0 percent of the 2023 total traffic respectively for the weekday a.m. and p.m. peak hours. The site traffic is estimated to be approximately 0.8 percent of the 2045 ITD corridor study projections.



## SH-44, Palmer Lane Intersection (MP 13) Widening - Conceptual Cost Estimate

Estimated By: Dan Block      Date: 6/04/2019  
 Checked By:      Date:  
 Location: SH-44, Palmer Lane Intersection (MP 13)

### Scope:

Provide a signalized intersection at SH-44 / Palmer Lane with lane configurations as shown in the SH-44 Corridor Study (KN 07827). This estimate assumes that KN 20266 will build out the "existing" (future) condition to a typical section of two lanes in each direction with 14' median, which is proposed by the SH-44 Corridor Study (KN 07827). All quantities are based on changes beyond what will be provided by the KN 20266 future condition. The widening of SH-44 to accommodate the additional left and right turn lanes will fit within the proposed 150-FT right-of-way footprint provided by KN 20266. Purchase of additional right-of-way will be required to accommodate the proposed widening of Palmer Lane. Storage lengths for turn lanes were determined based on 2045 traffic projections from the SH-44 Corridor Study (KN 07827), with the exception of the SB right lane which is 400' (including taper) per direction from ACHD. Some costs for unique, and/or highly variable pay items, are estimated using the descriptor ROM (Rough Order of Magnitude).

### SH-44 Existing Condition

- EB Median = 7-FT      • WB Median = 7-FT
- EB Lane = 12-FT      • WB Lane = 12-FT
- EB Lane = 12-FT      • WB Lane = 12-FT

### SH-44 Proposed Condition

- EB Left = 12-FT      • WB Left = 12-FT
- EB Through = 12-FT      • WB Through = 12-FT
- EB Through = 12-FT      • WB Through = 12-FT
- EB Right = 12-FT      • WB Right = 12-FT

### Palmer Lane Existing Condition

- NB Lane = 12-FT      • SB Lane = 12-FT
- NB Shoulder = 3-FT      • SB Shoulder = 3-FT

### Palmer Lane Proposed Condition

- NB Left = 12-FT      • SB Left = 12-FT
- NB Through = 12-FT      • SB Through = 12-FT
- NB Right = 12-FT      • SB Right = 12-FT

| Item   | Quantity    | Unit Price    | Cost                   | Note               |
|--|-------------|---------------|------------------------|--------------------|
| <b>SECTION 1</b>                                 |             |               |                        |                    |
| 201-010A CLEARING & GRUBBING                     | 1.00 LS     | \$ 5,000.00   | \$ 5,000.00            | ROM                |
| 203-130A REMOVAL OF PAVEMENT MARKINGS            | 4848.00 FT  | \$ 1.00       | \$ 4,848.00            | KN 13946           |
| 205-005A EXCAVATION (incl. saw-cutting)          | 260.11 CY   | \$ 39.50      | \$ 10,274.48           | KN 19415           |
| 205-060A WATER FOR DUST ABATEMENT                | 100.00 MG   | \$ 15.00      | \$ 1,500.00            | KN 18852           |
| 301-005A GRANULAR SUBBASE                        | 3676.00 TON | \$ 15.90      | \$ 58,448.40           | KN 18872           |
| 303-021A 3/4" AGGR TY A FOR BASE                 | 2609.00 TON | \$ 21.33      | \$ 55,649.97           | KN 20797           |
| 401-020A CSS-1 DIL EMUL ASPH FOR TACK COAT       | 663.70 GAL  | \$ 5.00       | \$ 3,318.50            | KN 13389           |
| 405-435A SUPERPAVE HMA PAV INCL ASPH&ADD CL SP-3 | 1780.00 TON | \$ 82.92      | \$ 147,597.60          | KN 13481           |
| 614-015A SIDEWALK                                | 261.00 SY   | \$ 62.00      | \$ 16,182.00           | KN 19965           |
| 614-025A CURB RAMP                               | 40.00 SY    | \$ 205.00     | \$ 8,200.00            | KN 19965           |
| 615-492A CURB & GUTTER TYPE 2                    | 251.00 FT   | \$ 31.85      | \$ 7,994.35            | KN 19965           |
| 621-005A SEED BED PREPARATION                    | 0.53 ACRE   | \$ 6,750.00   | \$ 3,577.50            | KN 13951           |
| 621-010A SEEDING                                 | 0.53 ACRE   | \$ 1,400.00   | \$ 742.00              | KN 13951           |
| 630-025A LONGITUDINAL PAVEMENT MARKING           | 12009.00 FT | \$ 0.35       | \$ 4,203.15            | KN 20797           |
| 656-005A TRAF SIGNAL INSTALLATION                | 1.00 LS     | \$ 309,833.33 | \$ 309,833.33          | KN 19814           |
| 675-005A SURVEY                                  | 1.00 LS     | \$ 30,000.00  | \$ 30,000.00           | ROM basis KN 18852 |
| SP RELOCATE MINOR IRRIGATION                     | 1.00 LS     | \$ 15,000.00  | \$ 15,000.00           | ROM                |
| Traffic Control                                  | 1.00 LS     | \$ 45,000.00  | \$ 45,000.00           | ROM basis KN 18852 |
| Miscellaneous Minor Items                        | 5%          |               | \$ 36,368.46           |                    |
| Mobilization                                     | 10%         |               | \$ 72,736.93           |                    |
| SECTION 1 Sub-Total                              |             |               | \$ 836,474.67          |                    |
| <b>SECTION 2</b>                                 |             |               |                        |                    |
| CN Change Order / Quantity Variance              | 5%          |               | \$ 41,823.73           |                    |
| CN Non-Bid Items                                 | 3.5%        |               | \$ 29,276.61           |                    |
| SECTION 2 Sub-Total                              |             |               | \$ 71,100.35           |                    |
| <b>SUMMARY</b>                                   |             |               |                        |                    |
| Sub-Total: SECTION 1 & SECTION 2                 |             |               | \$ 907,575.02          |                    |
| Contingency - Scoping Level                      | 30%         |               | \$ 272,272.51          |                    |
| <b>TOTAL CONSTRUCTION COST</b>                   |             |               | <b>\$ 1,179,847.52</b> |                    |

See Next Page for Summary of Project Costs ➡

### SH-44, Palmer Lane Intersection (MP 13) Widening - Conceptual Cost Estimate

| Summary of Project Costs |            | Amount          | Note |
|--------------------------|------------|-----------------|------|
| Construction             |            | \$ 1,179,847.52 |      |
| Design Services          | 10%        | \$ 117,984.75   |      |
| Construction Services    | 12%        | \$ 141,581.70   |      |
| Right-of-Way             | 0.40 ACRES | \$ 40,000.00    | ROM  |
| Total Cost               |            | \$ 1,479,413.98 |      |

### SH-44, Palmer Lane Intersection (MP 13) Widening - Concept



## Appendix B

### SH-44 / Short Road Documentation

**Table 4. Existing Intersection Operations – Weekday AM and PM Peak Hour**

| Intersection       | Intersection Control | Intersection Delay (sec), AM / PM | Lane Group | AM Peak Hour     |                  |                    | PM Peak Hour     |                  |                    |
|--------------------|----------------------|-----------------------------------|------------|------------------|------------------|--------------------|------------------|------------------|--------------------|
|                    |                      |                                   |            | V/C <sup>1</sup> | LOS <sup>2</sup> | Delay <sup>3</sup> | V/C <sup>1</sup> | LOS <sup>2</sup> | Delay <sup>3</sup> |
| SH 44 / Short Road | TWSC                 | N/A                               | EBTR       | -                | -                | -                  | -                | -                | -                  |
|                    |                      |                                   | WBL        | 0.02             | B                | 11.6               | 0.00             | A                | 9.2                |
|                    |                      |                                   | WBT        | -                | -                | -                  | -                | -                | -                  |
|                    |                      |                                   | NBL        | 0.04             | C                | 24.4               | 0.27             | D                | 34.6               |
|                    |                      |                                   | NBR        | 0.07             | C                | 23.1               | 0.05             | B                | 14.2               |

**Table 9. Year 2023 Background Intersection Operations – Weekday AM and PM Peak Hour**

| Intersection       | Intersection Control        | Intersection Delay (sec), AM / PM | Lane Group | AM Peak Hour     |                  |                    | PM Peak Hour     |                  |                    |
|--------------------|-----------------------------|-----------------------------------|------------|------------------|------------------|--------------------|------------------|------------------|--------------------|
|                    |                             |                                   |            | V/C <sup>1</sup> | LOS <sup>2</sup> | Delay <sup>3</sup> | V/C <sup>1</sup> | LOS <sup>2</sup> | Delay <sup>3</sup> |
| SH 44 / Short Road | TWSC<br>5 Lanes on SH 44    | N/A                               | EBT        | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | EBR        | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | WBL        | 0.06             | B                | 13.5               | 0.05             | B                | 10.1               |
|                    |                             |                                   | WBT        | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | NBL        | 0.24             | <b>E</b>         | <b>37.9</b>        | 0.28             | D                | 29.7               |
|                    |                             |                                   | NBR        | 0.10             | C                | 16.2               | 0.06             | B                | 11.9               |
|                    | TWSC<br>Ex 3 Lanes on SH 44 | N/A                               | EBT        | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | EBR        | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | WBL        | 0.06             | B                | 13.5               | 0.05             | B                | 10.1               |
|                    |                             |                                   | WBT        | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | NBL        | 0.24             | <b>E</b>         | <b>37.3</b>        | 0.46             | <b>F</b>         | <b>58.1</b>        |
|                    |                             |                                   | NBR        | 0.23             | D                | 34.7               | 0.09             | C                | 16.8               |

#### ***SH 44 (West State Street) / Short Road***

As highlighted in Table 9, the northbound left turning movement of the SH 44/Short Road intersection does not meet ITD operating standards under year 2023 background traffic conditions during the weekday a.m. with LOS E after the planned widening of SH 44. This is due to very high eastbound and westbound traffic volumes on SH 44, which result in very few gaps in traffic for left-turning and through movements on Short Road even though the volumes on Short Road are very low. Given a signal is already planned for the intersection of SH 44/Palmer Lane, signalization of this intersection was not evaluated because it would not meet ITD or ACHD spacing standards.

**Table 13. Year 2023 Total Intersection Operations – Weekday AM and PM Peak Hour**

| Intersection       | Intersection Control        | Intersection Delay (sec), AM / PM | Lane Group | AM Peak Hour     |                  |                    | PM Peak Hour     |                  |                    |
|--------------------|-----------------------------|-----------------------------------|------------|------------------|------------------|--------------------|------------------|------------------|--------------------|
|                    |                             |                                   |            | V/C <sup>1</sup> | LOS <sup>2</sup> | Delay <sup>3</sup> | V/C <sup>1</sup> | LOS <sup>2</sup> | Delay <sup>3</sup> |
| SH 44 / Short Road | TWSC<br>5 Lanes on SH 44    | N/A                               | EBTR       | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | WBL        | 0.08             | B                | 13.8               | 0.08             | B                | 10.4               |
|                    |                             |                                   | WBT        | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | NBL        | 0.44             | <b>E</b>         | <b>49.0</b>        | 0.39             | <b>E</b>         | <b>35.8</b>        |
|                    |                             |                                   | NBR        | 0.17             | C                | 17.2               | 0.08             | B                | 12.3               |
|                    | TWSC<br>Ex 3 Lanes on SH 44 | N/A                               | EBTR       | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | WBL        | 0.08             | B                | 13.8               | 0.08             | B                | 10.4               |
|                    |                             |                                   | WBT        | -                | -                | -                  | -                | -                | -                  |
|                    |                             |                                   | NBL        | 0.44             | <b>E</b>         | <b>48.0</b>        | 0.65             | <b>F</b>         | <b>83.8</b>        |
|                    |                             |                                   | NBR        | 0.39             | <b>E</b>         | <b>42.0</b>        | 0.14             | C                | 17.6               |

### SH 44 (West State Street) / Short Road

The northbound side street movement is projected to operate at LOS E but still have acceptable v/c ratios during the weekday a.m. and weekday p.m. peak hours. This is primarily due to the delay for the unsignalized northbound left-turn movement. Also shown are the operations without the ITD widening to 5 lanes, which are similar, but worse for the northbound right-turn movements. This is generally similar to 2023 background conditions.

Moon Valley Townhomes TIS

## SH 44 / Short Rd, Existing Volumes

EB Right Turn Lane

Traffic Manual: Idaho Supplementary Guidance to the MUTCD

Turn Lane Warrants

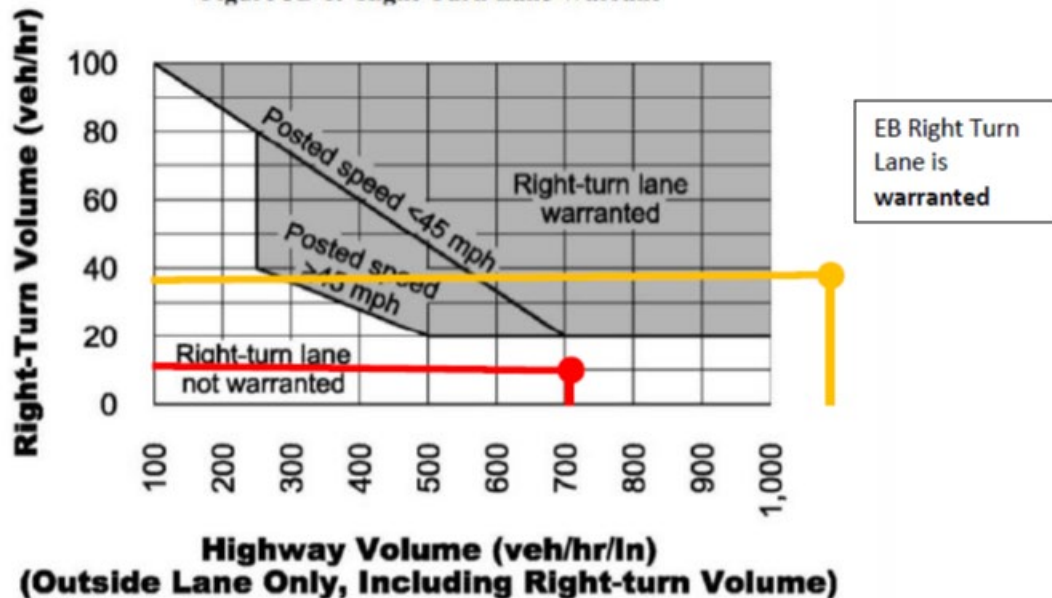
Legend

AM —

PM —

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**Figure 3B-1. Right-Turn Lane Warrant**



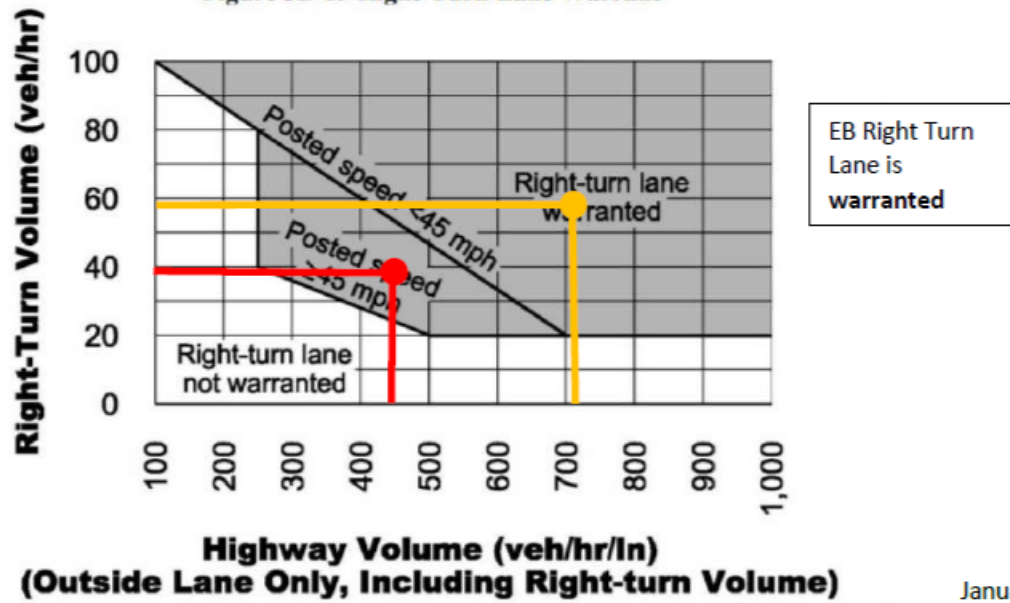
# SH 44 / Short Rd, 2023 Background Volumes (w/ SH 44 widening)

## EB Right Turn Lane

Traffic Manual: Idaho Supplementary Guidance to the MUTCD

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Figure 3B-1. Right-Turn Lane Warrant





## Intersection Widening - Conceptual Cost Estimate

Estimated By: **E. Bowen**      Date: **6/21/2021**  
 Checked By:                      Date:

Location: **Intersection of SH-44 and Short Road**

Scope: **Eastbound right turn lane including right-of-way**

| Item Description  | Quantity   | Unit Price    | Cost                 | Source   |
|---|------------|---------------|----------------------|----------|
| <b>SECTION 1</b>  |            |               |                      |          |
| 201-010A CLEARING & GRUBBING                                  | 0.28 ACRE  | \$ 6,833.00   | \$ 1,924.61          | KN 18872 |
| 203-015A REM OF BITUMINOUS SURF                               | 122.22 SY  | \$ 9.67       | \$ 1,181.89          | KN 18872 |
| 203-130A REMOVAL OF PAV MARKINGS                              | 0.00 FT    | \$ 1.00       | \$ -                 | KN 18852 |
| 205-005A EXCAVATION   | 407.41 CY  | \$ 15.25      | \$ 6,212.96          | KN 18872 |
| 301-005A GRANULAR SUBBASE                                     | 408.39 TON | \$ 15.90      | \$ 6,493.38          | KN 18872 |
| 303-022A 3/4" AGGR TY B FOR BASE                              | 388.36 TON | \$ 18.30      | \$ 7,106.91          | KN 18872 |
| 401-020A CSS-1 DIL EMUL ASPH FOR TACK COAT                    | 88.27 GAL  | \$ 3.48       | \$ 307.17            | KN 18872 |
| 405-435A SUPERPAVE HMA PAV INCL ASPH+ADD CL SP-3              | 236.67 TON | \$ 70.00      | \$ 16,566.55         | KN 18872 |
| 614-015A SIDEWALK   | 0.00 SY    | \$ 80.00      | \$ -                 | KN 20294 |
| 614-025A CURB RAMP  | 0.00 SY    | \$ 193.50     | \$ -                 | KN 19965 |
| 615-492A CURB & GUTTER TYPE 2                                 | 0.00 FT    | \$ 50.00      | \$ -                 | KN 20294 |
| 621-005A SEED BED PREPARATION                                 | 0.13 ACRE  | \$ 1,529.67   | \$ 199.08            | KN 18872 |
| 621-010A SEEDING  | 0.13 ACRE  | \$ 1,663.33   | \$ 216.48            | KN 18872 |
| 615-651A TRAFFIC SEPARATOR TYPE 1                             | 0.00 FT    | \$ 20.00      | \$ -                 | KN 19965 |
| 630-020A TRANSVERSE, WORD, SYMBOL, ARROW PAV MKG – WATERBORNE | 356.00 SF  | \$ 3.40       | \$ 1,210.40          | KN 13962 |
| 630-025A LONGITUDINAL PAVEMENT MARKING                        | 730.40 FT  | \$ 1.05       | \$ 766.92            | KN 18872 |
| 656-005A TRAF SIGNAL INSTALLATION                             | 0.00 LS    | \$ 229,000.00 | \$ -                 | KN 18872 |
| 675-005A SURVEY   | 1.00 LS    | \$ 10,000.00  | \$ 10,000.00         | KN 18872 |
| Traffic Control   | 1.00 LS    | \$ 45,000.00  | \$ 45,000.00         | KN 18872 |
| Miscellaneous Minor Items                                     | 5%         |               | \$ 4,859.32          |          |
| Mobilization  | 10%        |               | \$ 9,718.63          |          |
| <b>SECTION 1 Sub-Total</b>                                    |            |               | <b>\$ 111,764.30</b> |          |
| <b>SECTION 2</b>  |            |               |                      |          |
| CN Change Order / Quantity Variance                           | 5%         |               | \$ 5,588.22          |          |
| CN Non-Bid Items  | 3.5%       |               | \$ 3,911.75          |          |
| <b>SECTION 2 Sub-Total</b>                                    |            |               | <b>\$ 9,499.97</b>   |          |
| <b>SUMMARY</b>  |            |               |                      |          |
| Sub-Total: SECTION 1 & SECTION 2                              |            |               | \$ 121,264.27        |          |
| Contingency - Scoping Level                                   | 30%        |               | \$ 36,379.28         |          |
| <b>TOTAL CONSTRUCTION COST</b>                                |            |               | <b>\$ 157,643.55</b> |          |

| Summary of Project Costs |            | Amount               |
|--------------------------|------------|----------------------|
| Construction             |            | \$ 157,643.55        |
| Design Services          | 10%        | \$ 15,764.35         |
| Construction Services    | 12%        | \$ 18,917.23         |
| Right-of-Way             | 0.15 ACRES | \$ 15,000.00         |
| <b>Total Cost</b>        |            | <b>\$ 207,325.13</b> |



**Your Safety • Your Mobility  
Your Economic Opportunity**

**IDAHO TRANSPORTATION DEPARTMENT**

P.O. Box 8028 • Boise, ID 83707-2028  
(208) 334-8300 • [itd.idaho.gov](http://itd.idaho.gov)

June 21, 2021

Mark Tate  
M3 Companies  
1087 W. River Street, Suite 310  
Boise, ID 83702  
[MTate@m3companiesllc.com](mailto:MTate@m3companiesllc.com)  
Phone: 208.939.6263

VIA EMAIL

RE: Moon Valley Townhomes – ITD Development Condition Memo

Dear Mr. Tate,

The Idaho Transportation Department (ITD) appreciated the opportunity to review the Moon Valley Townhomes Traffic Impact Study (TIS) located on the southeast corner of SH-16 and SH-44. We have completed our review and although we do not have any technical questions, we do have concerns for the development's added trips to the intersections of SH-44 / Short Road and SH-44 / Palmer Lane.

ITD has entered into an agreement with the city of Star to collect a proportionate share contribution from each new development for impacts to the State highway system. As two agencies we are working together to accelerate highway construction within Star's area of impact to accommodate new development growth. ITD values your contribution to the transportation system so we can help keep goods, services and the public moving at an efficient pace.

Per the TIS, an eastbound right turn lane is warranted at the intersection of SH-44 / Short Rd. Since the improvement is at a public road intersection, ITD has estimated the cost of the improvement and calculated your development's proportionate share based off site traffic volumes versus total right turn lane traffic volumes at 2023 buildout.

ITD programmed the SH-44 / Palmer Lane signalization project for 2027 construction with the understanding that the department would request proportionate share contributions from new nearby developments to help accelerate the project. ITD calculated your development's proportionate share based off Palmer Lane site trips versus total 2045 Palmer Lane site trips.



**Your Safety • Your Mobility  
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P.O. Box 8028 • Boise, ID 83707-2028  
(208) 334-8300 • [itd.idaho.gov](http://itd.idaho.gov)

ITD determined Moon Valley Townhomes proportionate share contribution as the following. Details of the proportionate share calculation are included in the attached *ITD Staff Technical Report*.

| Intersection                                 | Proportionate Share Contribution |
|--|----------------------------------|
| SH-44 / Short Road                           | \$52,039                         |
| SH-44 / Palmer Lane                          | \$14,350                         |
| SH-16 / SH-44                                | N/A                              |
| <b>TOTAL</b>                                 | <b>\$66,389</b>                  |
| <b><i>Approximate per household unit</i></b> | <b><i>\$426</i></b>              |

Maintaining safety and mobility for Idaho's motorists is of utmost importance to ITD. We appreciate your improvements to livability in Star, ID as we want all residents to travel safely and efficiently around the Treasure Valley. If you have any questions please contact me by email at [jayme.coonce@itd.idaho.gov](mailto:jayme.coonce@itd.idaho.gov) or 208-334-8302.

Sincerely,

Jayme Coonce, P.E.  
ITD – District 3  
Engineer Manager

Cc:  
Shawn Nickel – City of Star  
Paige Bankhead – ACHD  
John Ringert – Kittelson & Associates

May 28, 2021

Mayor Trevor Chadwick  
City of Star  
P.O. Box 130  
Star, ID 83669

**Re: Moon Valley Townhomes Subdivision Preliminary Plat Application**

Dear Mayor:

Keller Associates, Inc. has reviewed the Preliminary Plat for the Moon Valley Townhomes Subdivision dated May 3, 2021. We reviewed the applicant's package to check conformance with the City's Subdivision Ordinance and coordinated our review with Shawn L. Nickel. We have the following comments based on our review.

1. No landscape plan is provided, landscape plans including fencing, buffer areas, and street trees will have to conform to the City subdivision ordinance.
2. Street lights are required at all intersections. Street lighting shall be in accordance with ISPWC and the City of Star Supplementals. Cut sheet for lights and light poles shall be approved in writing by the City prior to installation.
3. Some bearings shown on the plans indicate opposite directions from those in the legal description (e.g. NE vs SW), please rectify.
4. Plat note #2 references Canyon County Highway District, please clarify if this is CCHD or ACHD because the project is located in Ada County.
5. Plat references several times that Lot 1 includes a parking lot, is this a parking lot or private roads.
6. Drive isles shall be limited to 150 feet from the center of the perpendicular drive isle to the end dead end, or a fire turnaround is required, several isles appear to exceed this requirement.
7. Potable water cannot be used for irrigation purposes. A separate pressure irrigation system will be required. Construction plans for a subdivision-wide pressure irrigation system will be required for each final plat. Plan approvals and license agreements from the affected irrigation and/or canal companies will be required.
8. Historic irrigation lateral, drain, and ditch flow patterns shall be maintained unless approved in writing by the local irrigation district or ditch company.
9. Finish grades at subdivision boundaries shall match existing finish grades. Runoff shall be maintained on subdivision property unless otherwise approved.



10. 10-foot easements for pressure irrigation lines will need to be shown once the applicant determines the alignment location(s) for the facilities. Show all ditch and drainage easements.

11. Easements for sewer/water facilities will be required where placed outside of public right of way.

We recommend that the **conditions 1–6 listed above be addressed prior to approval of the Preliminary Plat**. Any variance or waivers to the City of Star standards, ordinances, or policies must be specifically approved in writing by the City. Approval of the above-referenced Preliminary Plat does not relieve the Registered Professional Land Surveyor or the Registered Professional Engineer of those responsibilities.

If you have any questions, please do not hesitate to call Keller Associates at (208) 288-1992.

Sincerely,

**KELLER ASSOCIATES, INC.**

A handwritten signature in blue ink, appearing to read "Ryan V. Morgan", is written over a horizontal line.

Ryan V. Morgan, P.E.  
City Engineer

cc: File

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MIDDLETON RURAL FIRE DISTRICT

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Moon Valley Townhomes Subdivision

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STAR FIRE PROTECTION DISTRICT

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DATE: August 30, 2021

TO: City of Star, Planning & Zoning

FROM: Victor Islas, Deputy Chief

SUBJECT: Fire District Review

PROJECT NAME: Moon Valley Townhomes Subdivision (PP-21-12, DA-21-13-MOD)

**Fire District Summary Report:**

**Overview:** This development can be serviced by the Star Fire Protection District. This development shall comply with the 2018 International Fire Code (IFC) and any codes set forth by the City of Star, Idaho.

**Fire Response Time:** This development will be served by the Star Fire Protection District Station 51, located at 11655 W. State St., Star, Idaho. Station 51 is 3 miles with a travel time of 8 minutes under ideal driving conditions.

**Accessibility:** Roadway Access, Traffic, Radio Coverage

Access roads shall be provided and maintained following Appendix D and Section 503 of the IFC. Access shall include adequate roadway widths, signage, turnarounds, and turning radius for fire apparatus.

Access road design shall be designed and constructed to allow for evacuation simultaneously with emergency response operations.

All access roads in this development shall remain clear and unobstructed during construction of the development. Additional parking restrictions may be required as to always maintain access for emergency vehicles. Hydrants shall always remain unobstructed per city code.

One- or two-family dwelling residential developments: Developments of one- or two-family dwellings where the number of dwelling units exceeds 30 shall provide with at least two separate and approved fire apparatus access roads.

*Review Note: The Fire District would like to meet with the developer to discuss the current access that is planned.*

Residential structures that sit more than 150ft off the roadway will require additional turnaround for emergency access.

The fire district requires that Autoturn models be submitted for review. Autoturn models should be reflect the utilization of a 36' long fire engine and a 50' long ladder truck.

Traffic calming devices will require approval by the Fire District.

An unobstructed vertical clearance of no less than 13 feet 6 inches shall be always maintained.

(208) 286-7772  
11665 W. STATE ST., SUITE B  
STAR, IDAHO 83669

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MIDDLETON RURAL FIRE DISTRICT

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Moon Valley Townhomes Subdivision

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STAR FIRE PROTECTION DISTRICT

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It shall be the responsibility of the developer and or HOA to maintain clearance of access roads all year around. This shall include but not limited to snow removal and vehicles.

No street parking shall be allowed. No parking signs to be posted as per IFC D103.6.

The applicant shall work with City of Star, Ada County and Fire District to provide an address identification plan and signage which meets the requirements set forth by each agency. Addressing shall be placed in a position that is plainly legible and visible from the street or road fronting the property, as set forth in International Fire Code Section 505.1

Upon commencement of initial construction of a new structure, a clear visible freestanding sign or post shall be erected and maintained in place until the permanent address numerals are attached or otherwise displaced upon the premises at completion.

Electronic gates will require Knox key switch and yelp activation.

*Review Note: During our review it has been noted that the access to the proposed development site is limited to W. Moon Valley Rd. via S. Short Rd and N. Palmer Ln. With the addition of this development this will add additional traffic to the area which could in turn decrease response time to emergent and non-emergent calls for service.*

**Water Supply:**

Water supply requirements will be followed as described in Appendix B of the 2018 International Fire Code unless agreed upon by the Fire District.

1. Fire Flow: One- and two-family dwellings not exceeding 3,600 square feet require a fire-flow of 1,000 gallons per minute for a duration of 1 hours to service the entire project. One- and two-family dwellings in excess of 3,600 square feet require a minimum fire flow as specified in Appendix B of the International Fire Code.
2. Water Supply: Acceptance of the water supply for fire protection will be by the Fire District and water quality by the Star Sewer & Water District for bacteria testing.
3. Water Supply: Final Approval of the fire hydrant locations shall be by the Star Fire Protection District or their designee in accordance with International Fire Code Section (IFC) 508.5.4 as follows:
  - a. Fire hydrants shall have a Storz LDH connection in place of the 4 ½" outlet. The Storz connection may be integrated into the hydrant, or an approved adapter may be used on the 4 1/2" outlet.
  - b. Fire hydrants shall have the Storz outlet face the main street or parking lot drive aisle.
  - c. Fire hydrants shall be placed on corners when spacing permits.
  - d. Fire hydrants shall not have any vertical obstructions to outlets within 10'.
  - e. Fire hydrants shall be placed 18" above finished grade to the center of the Storz outlet.
  - f. Fire hydrants shall be provided to meet the requirements of the City of Star and Star Sewer and Water District Standards.
  - g. Show all proposed or existing hydrants for all new construction or additions to existing buildings within 1,000 feet of the project.

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MIDDLETON RURAL FIRE DISTRICT

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Moon Valley Townhomes Subdivision

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STAR FIRE PROTECTION DISTRICT

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**Inspections:**

Final inspection by the Fire District of the above listed including hydrant flow must be completed before building permits are issued

**Additional Comments:**

Side Setback as per City Code. Any modification to setback will require review and approval by the Fire District.

Streetlights shall be turned on once residential building begins, Lighting is essential in assisting first responders with identifying entrances safely while responding to calls for service.

Additional life safety review and permits will be required for commercial and residential buildings prior to construction.





**STATE OF IDAHO**  
DEPARTMENT OF  
ENVIRONMENTAL QUALITY

1445 N Orchard Street, Boise, ID 83706  
(208) 373-0550

Brad Little, Governor  
Jess Byrne, Director

May 21, 2021

By e-mail: [snickel@staridaho.org](mailto:snickel@staridaho.org)

City of Star  
P.O. Box 130  
Star, Idaho 83669

Subject: Moon Valley Townhomes Subdivision, PP-21-12/DA-21-13-MOD

Dear Mr. Nickel:

Thank you for the opportunity to respond to your request for comment. While DEQ does not review projects on a project-specific basis, we attempt to provide the best review of the information provided. DEQ encourages agencies to review and utilize the Idaho Environmental Guide to assist in addressing project-specific conditions that may apply. This guide can be found at: [deq.idaho.gov/assistance-resources/environmental-guide-for-local-govts](https://deq.idaho.gov/assistance-resources/environmental-guide-for-local-govts).

The following information does not cover every aspect of this project; however, we have the following general comments to use as appropriate:

**1. AIR QUALITY**

- Please review IDAPA 58.01.01 for all rules on Air Quality, especially those regarding fugitive dust (58.01.01.651), trade waste burning (58.01.01.600-617), and odor control plans (58.01.01.776).
- All property owners, developers, and their contractor(s) must ensure that reasonable controls to prevent fugitive dust from becoming airborne are utilized during all phases of construction activities per IDAPA 58.01.01.651.
- DEQ recommends the city/county require the development and submittal of a dust prevention and control plan for all construction projects prior to final plat approval. Dust prevention and control plans incorporate appropriate best management practices to control fugitive dust that may be generated at sites.
- Citizen complaints received by DEQ regarding fugitive dust from development and construction activities approved by cities or counties will be referred to the city/county to address under their ordinances.

- Per IDAPA 58.01.01.600-617, the open burning of any construction waste is prohibited. The property owner, developer, and their contractor(s) are responsible for ensuring no prohibited open burning occurs during construction.
- For questions, contact David Luft, Air Quality Manager, at (208) 373-0550.

## **2. WASTEWATER AND RECYCLED WATER**

- DEQ recommends verifying that there is adequate sewer to serve this project prior to approval. Please contact the sewer provider for a capacity statement, declining balance report, and willingness to serve this project.
- IDAPA 58.01.16 and IDAPA 58.01.17 are the sections of Idaho rules regarding wastewater and recycled water. Please review these rules to determine whether this or future projects will require DEQ approval. IDAPA 58.01.03 is the section of Idaho rules regarding subsurface disposal of wastewater. Please review this rule to determine whether this or future projects will require permitting by the district health department.
- All projects for construction or modification of wastewater systems require preconstruction approval. Recycled water projects and subsurface disposal projects require separate permits as well.
- DEQ recommends that projects be served by existing approved wastewater collection systems or a centralized community wastewater system whenever possible. Please contact DEQ to discuss potential for development of a community treatment system along with best management practices for communities to protect ground water.
- DEQ recommends that cities and counties develop and use a comprehensive land use management plan, which includes the impacts of present and future wastewater management in this area. Please schedule a meeting with DEQ for further discussion and recommendations for plan development and implementation.

For questions, contact Valerie Greear, Water Quality Engineering Manager at (208) 373-0550.

## **3. DRINKING WATER**

- DEQ recommends verifying that there is adequate water to serve this project prior to approval. Please contact the water provider for a capacity statement, declining balance report, and willingness to serve this project.
- IDAPA 58.01.08 is the section of Idaho rules regarding public drinking water systems. Please review these rules to determine whether this or future projects will require DEQ approval.
- All projects for construction or modification of public drinking water systems require preconstruction approval.
- DEQ recommends verifying if the current and/or proposed drinking water system is a regulated public drinking water system (refer to the DEQ website at: [deg.idaho.gov/water-quality/drinking-water.aspx](http://deg.idaho.gov/water-quality/drinking-water.aspx)). For non-regulated systems, DEQ recommends annual testing for total coliform bacteria, nitrate, and nitrite.

- If any private wells will be included in this project, we recommend that they be tested for total coliform bacteria, nitrate, and nitrite prior to use and retested annually thereafter.
- DEQ recommends using an existing drinking water system whenever possible or construction of a new community drinking water system. Please contact DEQ to discuss this project and to explore options to both best serve the future residents of this development and provide for protection of ground water resources.
- DEQ recommends cities and counties develop and use a comprehensive land use management plan which addresses the present and future needs of this area for adequate, safe, and sustainable drinking water. Please schedule a meeting with DEQ for further discussion and recommendations for plan development and implementation.

For questions, contact Valerie Greear, Water Quality Engineering Manager at (208) 373-0550.

#### 4. SURFACE WATER

- Please contact DEQ to determine whether this project will require a National Pollution Discharge Elimination System (NPDES) Permit. A Construction General Permit from EPA may be required if this project will disturb one or more acres of land, or will disturb less than one acre of land but are part of a common plan of development or sale that will ultimately disturb one or more acres of land.
- If this project is near a source of surface water, DEQ requests that projects incorporate construction best management practices (BMPs) to assist in the protection of Idaho's water resources. Additionally, please contact DEQ to identify BMP alternatives and to determine whether this project is in an area with Total Maximum Daily Load stormwater permit conditions.
- The Idaho Stream Channel Protection Act requires a permit for most stream channel alterations. Please contact the Idaho Department of Water Resources (IDWR), Western Regional Office, at 2735 Airport Way, Boise, or call (208) 334-2190 for more information. Information is also available on the IDWR website at: <https://idwr.idaho.gov/streams/stream-channel-alteration-permits.html>
- The Federal Clean Water Act requires a permit for filling or dredging in waters of the United States. Please contact the US Army Corps of Engineers, Boise Field Office, at 10095 Emerald Street, Boise, or call 208-345-2155 for more information regarding permits.

For questions, contact Lance Holloway, Surface Water Manager, at (208) 373-0550.

#### 5. SOLID WASTE, HAZARDOUS WASTE AND GROUND WATER CONTAMINATION

- **Solid Waste.** No trash or other solid waste shall be buried, burned, or otherwise disposed of at the project site. These disposal methods are regulated by various state regulations including Idaho's Solid Waste Management Regulations and Standards (IDAPA 58.01.06), Rules and Regulations for Hazardous Waste (IDAPA 58.01.05), and Rules and Regulations for the Prevention of Air Pollution (IDAPA 58.01.01). Inert and other approved materials are also defined in the Solid Waste Management Regulations and Standards

- **Hazardous Waste.** The types and number of requirements that must be complied with under the federal Resource Conservation and Recovery Act (RCRA) and the Idaho Rules and Standards for Hazardous Waste (IDAPA 58.01.05) are based on the quantity and type of waste generated. Every business in Idaho is required to track the volume of waste generated, determine whether each type of waste is hazardous, and ensure that all wastes are properly disposed of according to federal, state, and local requirements.
- **Water Quality Standards.** Site activities must comply with the Idaho Water Quality Standards (IDAPA 58.01.02) regarding hazardous and deleterious-materials storage, disposal, or accumulation adjacent to or in the immediate vicinity of state waters (IDAPA 58.01.02.800); and the cleanup and reporting of oil-filled electrical equipment (IDAPA 58.01.02.849); hazardous materials (IDAPA 58.01.02.850); and used-oil and petroleum releases (IDAPA 58.01.02.851 and 852). Petroleum releases must be reported to DEQ in accordance with IDAPA 58.01.02.851.01 and 04. Hazardous material releases to state waters, or to land such that there is likelihood that it will enter state waters, must be reported to DEQ in accordance with IDAPA 58.01.02.850.
- **Ground Water Contamination.** DEQ requests that this project comply with Idaho's Ground Water Quality Rules (IDAPA 58.01.11), which states that "No person shall cause or allow the release, spilling, leaking, emission, discharge, escape, leaching, or disposal of a contaminant into the environment in a manner that causes a ground water quality standard to be exceeded, injures a beneficial use of ground water, or is not in accordance with a permit, consent order or applicable best management practice, best available method or best practical method."

For questions, contact Albert Crawshaw, Waste & Remediation Manager, at (208) 373-0550.

## 6. ADDITIONAL NOTES

- If an underground storage tank (UST) or an aboveground storage tank (AST) is identified at the site, the site should be evaluated to determine whether the UST is regulated by DEQ. EPA regulates ASTs. UST and AST sites should be assessed to determine whether there is potential soil and ground water contamination. Please call DEQ at (208) 373-0550, or visit the DEQ website [deq.idaho.gov/waste-mgmt-remediation/storage-tanks.aspx](http://deq.idaho.gov/waste-mgmt-remediation/storage-tanks.aspx) for assistance.
- If applicable to this project, DEQ recommends that BMPs be implemented for any of the following conditions: wash water from cleaning vehicles, fertilizers and pesticides, animal facilities, composted waste, and ponds. Please contact DEQ for more information on any of these conditions.



Response to Request for Comment

May 21, 2021

Page 5

We look forward to working with you in a proactive manner to address potential environmental impacts that may be within our regulatory authority. If you have any questions, please contact me, or any of our technical staff at (208) 373-0550.

Sincerely,

A handwritten signature in black ink that reads "Aaron Scheff". The signature is written in a cursive, flowing style.

Aaron Scheff  
Regional Administrator  
DEQ-Boise Regional Office

EDMS#: 2021AEK95



Ada County Transmittal  
Division of Community and Environmental Health

Return to:

- ☐ ACZ  
☐ Boise  
☐ Eagle  
☐ Garden City  
☐ Meridian  
☐ Kuna  
☒ Star

Rezone # \_\_\_\_\_

Conditional Use # \_\_\_\_\_

Preliminary / Final / Short Plat PP-21-12

Moon Valley Townhomes

- ☐ 1. We have No Objections to this Proposal.
- ☐ 2. We recommend Denial of this Proposal.
- ☐ 3. Specific knowledge as to the exact type of use must be provided before we can comment on this Proposal.
- ☐ 4. We will require more data concerning soil conditions on this Proposal before we can comment.
- ☐ 5. Before we can comment concerning individual sewage disposal, we will require more data concerning the depth of:  
☐ high seasonal ground water ☐ waste flow characteristics  
☐ bedrock from original grade ☐ other \_\_\_\_\_
- ☐ 6. This office may require a study to assess the impact of nutrients and pathogens to receiving ground waters and surface waters.
- ☐ 7. This project shall be reviewed by the Idaho Department of Water Resources concerning well construction and water availability.
- ☒ 8. After written approvals from appropriate entities are submitted, we can approve this proposal for:  
☒ central sewage ☐ community sewage system ☐ community water well  
☐ interim sewage ☒ central water  
☐ individual sewage ☐ individual water
- ☒ 9. The following plan(s) must be submitted to and approved by the Idaho Department of Environmental Quality:  
☒ central sewage ☐ community sewage system ☐ community water  
☐ sewage dry lines ☒ central water
- ☐ 10. This Department would recommend deferral until high seasonal ground water can be determined if other considerations indicate approval.
- ☐ 11. If restroom facilities are to be installed, then a sewage system MUST be installed to meet Idaho State Sewage Regulations.
- ☐ 12. We will require plans be submitted for a plan review for any:  
☐ food establishment ☐ swimming pools or spas ☐ child care center  
☐ beverage establishment ☐ grocery store
- ☒ 13. Infiltration beds for storm water disposal are considered shallow injection wells. An application and fee must be submitted to CDH.
- ☐ 14. \_\_\_\_\_

Reviewed By: Ron Boly

Date: 5/12/2021

**Project/File: Moon Valley Townhomes/ SPP21-0010/ PP-21-12**

This is a preliminary plat application to allow for the development of a 157-lot residential subdivision on 12-acres. The site is located southeast of Highway 16 and State Street.

**Lead Agency:** City of Star

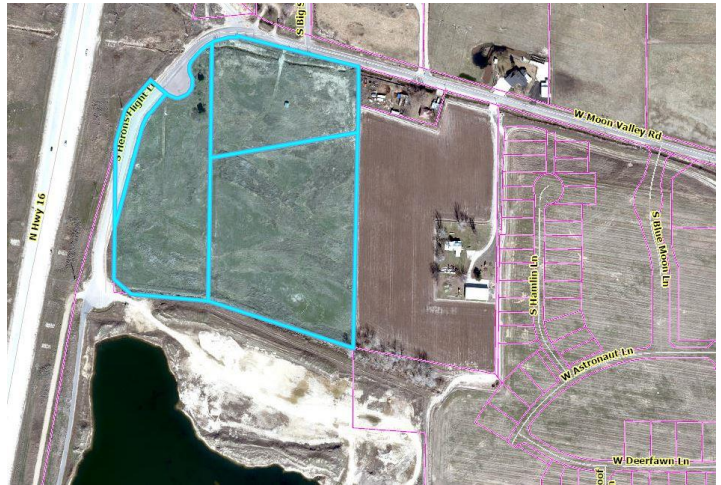
**Site address:** 8323 W. Moon Valley Road,  
S0416120627, S0416110207  
and S0416120646

**Staff Approval:** August 4, 2021

**Applicant:** Mark Tate  
M3 ID Moon Valley  
1087 W. River Stret, Ste. 310  
Boise, ID 83702

**Representative:** Wendy Shrief  
JUB Engineers  
2760 W. Excursion Lane, Ste 400  
Meridian, ID 83642

**Staff Contact:** Dawn Battles, Planner II  
Phone: 387-6218  
E-mail: [dbattles@achdidaho.org](mailto:dbattles@achdidaho.org)



## **A. Findings of Fact**

- Description of Application:** The applicant is requesting approval of a preliminary plat application to allow for the development of 157-lots (156 residential lots and 1 common lot) on a lot within the Moon Valley Commons Subdivision previously approved in 2019. This application also includes a modification to an existing development agreement with the City of Star to allow for attached single family homes to be constructed within the mixed-use zoning district. The applicant's proposal is not consistent with the City of Star's future land use map which designates this area as commercial.

- Description of Adjacent Surrounding Area:**

| Direction | Land Use                            | Zoning      |
|-----------|-------------------------------------|-------------|
| North     | Rural-Urban Transition (Ada County) | RUT         |
| South     | Compact/High-Density Residential    | R-8         |
| East      | Compact/High-Density Residential    | R-8         |
| West      | Commercial                          | C-1 and CBD |

- Site History:** ACHD staff has previously reviewed a portion of this site as Moon Valley Commons Subdivision/SPP20-0002/ PP-20-05 consisting of 93 residential lots, 34 common lots, 1 commercial

lot and private roads in March 2020. The requirements of this staff report are consistent with those of the prior action.

**4. Adjacent Development:** The following developments are pending or underway in the vicinity of the site:

- Moon Valley Subdivision, consisting of 268 single family building lots, 22 common lots, 8 driveway lots and a private road on 145-acres located south and southeast of the site was approved by ACHD in March 2019.

**5. Transit:** Transit services are not available to serve this site.

**6. Pathway Crossings:** United States Access Board R304.5.1.2 Shared Use Paths. In shared use paths, the width of curb ramps runs and blended transitions shall be equal to the width of the shared use path.

AASHTO's Guidelines for the Development of Bicycle Facilities 5.3.5 Other Intersection Treatments: The opening of a shared use path at the roadway should be at least the same width as the shared use path itself. If a curb ramp is provided, the ramp should be the full width of the path, not including any flared sides if utilized. . . . Detectable warnings should be placed across the full width of the ramp.

FHWA's "Designing Sidewalks and Trails for Access" (1999) reflected common ADA-related concepts: Chapter 6, Page 16-6: The width of the ramp should be at least as wide as the average width of the trail to improve safety for users who will be traveling at various speeds. In addition, the overall width of the trail should be increased, so the curb ramp can be slightly offset to the side. The increased width reduces conflict at the intersection by providing more space for users at the bottom of the ramp.

**7. New Center Lane Miles:** The proposed development includes 0 centerline miles of new public road.

**8. Impact Fees:** There will be an impact fee that is assessed and due prior to issuance of any building permits. The assessed impact fee will be based on the impact fee ordinance that is in effect at that time. The impact fee assessment will not be released until the civil plans are approved by ACHD.

**9. Capital Improvements Plan (CIP)/ Integrated Five Year Work Plan (IFYWP):**

There are no roadways, bridges or intersections in the general vicinity of the project that are in the Integrated Five Year Work Plan (IFYWP) or the District's Capital Improvement Plan (CIP).

**10. Roadways to Bikeways Master Plan:** ACHD's Roadways to Bikeways Master Plan (BMP) was adopted by the ACHD Commission in May of 2009 and was update in 2018. The plan seeks to implement the Planned Bicycle Network to support bicycling as a viable transportation option for Ada County residents with a wide range of ages and abilities, maintain bicycle routes in a state of good repair in order to ensure they are consistently available for use, promote awareness of existing bicycle routes and features and support encouragement programs and to facilitate coordination and cooperation among local jurisdictions in implementing the Roadways to Bikeways Plan recommendations.

The BMP identifies Moon Valley Road, east of the site, as a Level 2 facility that will be constructed as part of a future ACHD project.

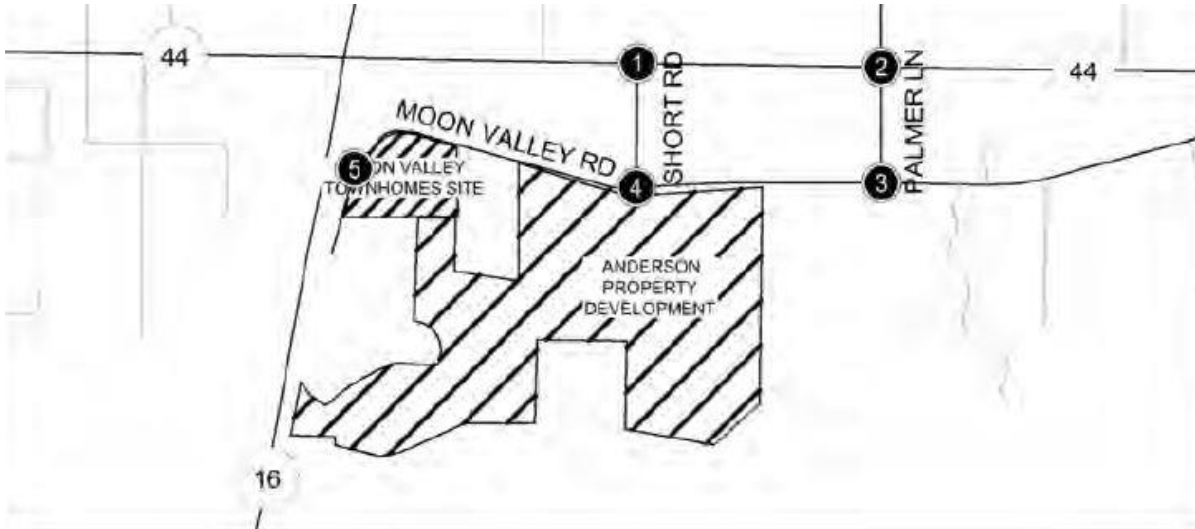
## **B. Traffic Findings for Consideration**

**1. Trip Generation:** This development is estimated to generate 1,139 vehicle trips per day; 88 vehicle trips per hour in the PM peak hour, based on the traffic impact study.

**2. Traffic Impact Study**



Kittleson & Associates prepared a traffic impact study for the proposed Moon Valley Townhomes. The executive summary of the findings **as presented by Kittelson & Associates** can be found as Attachment 3. ACHD has reviewed the submitted traffic impact study for consistency with ACHD policies and practices and may have additional requirements beyond what is noted in the summary. ACHD Staff comments on the submitted traffic impact study can be found below under staff comments.



Study Areas

**a. Policy:**

**Mitigation Proposals:** Mitigation recommendations shall be provided within the report. At a minimum, for each roadway segment and intersection that does not meet the minimum acceptable level of service planning threshold or v/c ratio, the report must discuss feasible measures to avoid or reduce the impact to the system. To be considered adequate, measures should be specific and feasible. Mitigation may also include:

- Revision to the Phasing Plan to coincide with the District's planning Capital Projects.
- Reducing the scope and/or scale of the project.

**Alternative Mitigation Measures:** 7106.7.3 states that if traditional mitigation measures such as roadway widening and intersection improvements are infeasible as determined by ACHD, the TIS may recommend alternative mitigation measures. Alternative mitigation measures shall demonstrate that impacts from the project will be offset.

- If the impacted roadway segments and/or intersections are programmed as funded in the Integrated Five Year Work Plan (IFYWP) or the Capital Improvements Plan (CIP); no alternative mitigation is required.
- If the impacted roadway segments and/or intersections are not programmed in either the IFYWP or the CIP; the applicant may (i) analyze the shoulder hour and (ii) provide a safety analysis to determine alternative mitigation requirements.
  - If the impacted roadway segments and intersections meet the minimum acceptable level of service planning thresholds in the shoulder hour the applicant may suggest feasible alternative mitigation such as: sidewalks, bike facilities, connectivity, safety improvements, etc. within 1.5 miles of the proposed development.

- If the shoulder hour planning thresholds are exceeded the applicant may request to enter into a Development Agreement and pay into the Priority Corridor Fund an amount determined by the ACHD to offset impacts from the project.
- Alternative Mitigation may also include:
  - Revision to the Phasing Plan to coincide with the District's future Capital Projects.
  - Reducing the scope and/or scale of the project.

**Level of Service Planning Thresholds:** District Policy 7206.4.1 states that, Level of Service Planning Thresholds have been established for principal arterials and minor arterials within ACHD's Capital Improvement Plan and are also listed in section 7106. Unless otherwise required to provide a Traffic Impact Study under section 7106, a proposed development with site traffic less than 10% of the existing downstream roadway or intersection peak hour traffic shall not be required to provide mitigation for a roadway or intersection that currently exceeds the minimum acceptable level of service planning threshold or V/C ratio.

- b. Staff Comments/Recommendations:** The TIS indicates that all roadway segments in the study area are expected to operate at an acceptable level of service planning threshold during the AM and PM peak hours under existing traffic, 2023 background traffic and 2023 total traffic conditions. The TIS also notes that the intersections in the study area are expected to operate at an acceptable level of service planning threshold in the AM and PM peak hours under existing traffic conditions with the exception of SH-44/Palmer Lane.

The TIS recommends mitigation measures for the following intersections to mitigate existing traffic, 2023 background traffic and 2023 total traffic conditions. The percentage of site generated traffic under total traffic conditions (2023) in the PM peak hour is provided in parenthesis.

**Intersections:**

- SH-44/Palmer Lane (2%)
  - Signal warrant analysis indicated that the intersection meets warrants under existing traffic, 2023 background and 2023 total traffic conditions.

The critical movements at the intersection are expected to operate as follows:

- Existing Conditions—LOS F in the PM peak hours (northbound left and through-right) and LOS E in the PM peak hours (southbound left and through-right).
- 2023 Background Conditions—LOS F in the AM and PM peak hours (southbound left and through-right) with 3-lanes on SH-44. LOS E in the PM peak hours (southbound left and through-right) with 5-lanes on SH-44.
- 2023 Total Traffic Conditions—LOS F in the AM and PM peak hours (southbound left and through-right) with 3-lanes on SH-44. LOS E in the PM peak hours (southbound left and through-right) with 5-lanes on SH-44.

Consistent with ACHD policy, a signal warrant analysis was completed for the intersection. The intersection analysis found that the intersection meets the 8 and 4 hour peak hour warrants.

This intersection is located off-site, and Palmer Lane is not scheduled for any improvements in the CIP or the IFYWP. In addition, the proposed site traffic at this intersection will also be less than 10% of the 2023 total traffic conditions. Therefore, consistent with District policy 7205.3.1 Level of Service Planning Threshold, which states, a proposed development with site traffic less

than 10% of the existing downstream roadway or intersection peak hour traffic shall not be required to provide mitigation; no improvements are required at this intersection.

The TIS also notes under total traffic conditions that this intersection is expected to operate at an acceptable level of service planning threshold in the AM and PM peak hours as a signalized intersection with 5-lanes on SH-44.

See Findings for Consideration 1 for ITD's comments on this intersection.

- SH-44/Short Road (3.1%)
  - Northbound left movement exceeds ITD's threshold for 2023 background and 2023 total traffic conditions with 3-lanes and 5-lanes on SH-44
  - Eastbound right-turn lane warranted on SH-44 under existing traffic and 2023 background traffic conditions with 5-lanes on SH-44.

The TIS notes signalization of this intersection was not evaluated because it would not meet ITD or ACHD spacing standards due to a signal being planned for the SH-44/Palmer Lane intersection.

This intersection is located off-site, and Short Road is not scheduled for any improvements in the CIP or the IFYWP. In addition, the proposed site traffic at this intersection will also be less than 10% of the 2023 total traffic conditions. Therefore, consistent with District policy 7205.3.1 Level of Service Planning Threshold, which states, a proposed development with site traffic less than 10% of the existing downstream roadway or intersection peak hour traffic shall not be required to provide mitigation; no improvements are required at this intersection.

See Findings for Consideration 1 for ITD's comments on this intersection.

### **Site Access Evaluation**

- Site Access 5, onto the public portion of Herons Flight Lane, a proposed full access driveway located approximately 2,200-feet west of Short Road was evaluated and identified that intersection sight distance was insufficient. The TIS notes the speed limit on Moon Valley Road should be reduced to 20 miles per hour. However, staff is not agreeable to the reduced speed limit and therefore the applicant revised the site plan and relocated the access approximately 100-feet south, onto the private portion of Herons Flight Lane, to obtain adequate intersection sight distance. While the intersection sight distance is still below the posted speed limit, the TIS notes the driveway should be approved as there will be few, if any, westbound left-turning movements, and slower speeds will be obtained due to the roadway terminating in a cul-de-sac to the south. Additionally, to ensure adequate sight distance, the existing vegetation along Heron Flight Lane, north of the proposed site access, should be trimmed and maintained. Staff recommends approval of the driveway onto the private portion of Herons Flight Lane to be located approximately 2,300-feet west of Short Road as it is needed to serve the site.

### **Local Roadway Volumes-Short Road**

The TIS notes that the average daily traffic count for Short Road south of SH-44 is 1,065 trips per day. With the additional traffic anticipated to be generated by the development, the average daily traffic count for Short Road is anticipated to increase to 3,380. ACHD policy allows a threshold of 2,000 trips per day on local streets. The TIS recommends the reclassification of Short Road to a collector roadway. Although Short Road is classified as a local street, it functions as a commercial local street due to the existing church abutting the site and the approved commercial site within the Moon Valley Commons development. ACHD policy allows up to 8,500 average daily trips (ADT) per day on a local commercial street, negating the concerns regarding ADT on Short Road.

## **3. Condition of Area Roadways**

Traffic Count is based on Vehicles per hour (VPH)

| Roadway                                      | Frontage | Functional Classification | PM Peak Hour Traffic Count | PM Peak Hour Level of Service |
|--|----------|---------------------------|----------------------------|-------------------------------|
| Moon Valley Road                             | 445-feet | Collector                 | 51                         | Better than "D"               |
| Moon Valley Road (Palmer Lane to Short Road) | None     | Collector                 | 9                          | Better than "D"               |
| Palmer Lane                                  | None     | Collector                 | 18                         | Better than "D"               |
| Short Road                                   | None     | Local                     | 49                         | N/A                           |

\* Acceptable level of service for a two-lane collector is "D" (425 VPH).

#### 4. Average Daily Traffic Count (VDT)

*Average daily traffic counts are based on ACHD's most current traffic counts.*

- The average daily traffic count for Moon Valley Road west of Short Road was 116 on December 9, 2020.
- The average daily traffic count for Moon Valley Road between Palmer Lane to Short Road was 268 on December 9, 2020.
- The average daily traffic count for Palmer Lane south of SH-44 was 729 on December 9, 2020.
- The average daily traffic count for Short Road south of SH-44 was 1,065 on December 9, 2020.

## C. Findings for Consideration

### 1. State Highway SH-44 /State Street

SH-44 is under the jurisdiction of the Idaho Transportation Department (ITD). The applicant, City of Star, and ITD should work together to determine if additional right-of-way or improvements are necessary on SH-44.

**Staff Comments/Recommendations:** On June 21, 2021, ITD issued a comment letter on the traffic impact study prepared for the Moon Valley Townhomes subdivision, see Attachment 4.

ITD has requested that the City of Star require the developer to pay their proportionate share of \$66,389 towards future improvements programmed by ITD and mitigation required as part of the TIS at the following intersections:

- SH-44/Short Road (\$52,039)
- Short Road/Palmer Lane (\$14,350)

### 2. Moon Valley Road

a. **Existing Conditions:** Moon Valley Road is improved with 2-travel lanes, 25-feet of pavement and no curb, gutter or sidewalk abutting the site. There is 55 to 75-feet of right-of-way for Moon Valley Road (17 to 23-feet from centerline). There is prescriptive right-of-way abutting the site along Moon Valley Road.

b. **Policy:**



**Collector Street Policy:** District policy 7206.2.1 states that the developer is responsible for improving all collector frontages adjacent to the site or internal to the development as required below, regardless of whether access is taken to all of the adjacent streets.

**Master Street Map and Typologies Policy:** District policy 7206.5 states that if the collector street is designated with a typology on the Master Street Map, that typology shall be considered for the required street improvements. If there is no typology listed in the Master Street Map, then standard street sections shall serve as the default.

**Street Section and Right-of-Way Policy:** District policy 7206.5.2 states that the standard right-of-way width for collector streets shall typically be 50 to 70-feet, depending on the location and width of the sidewalk and the location and use of the roadway. The right-of-way width may be reduced, with District approval, if the sidewalk is located within an easement; in which case the District will require a minimum right-of-way width that extends 2-feet behind the back-of-curb on each side.

The standard street section shall be 46-feet (back-of-curb to back-of-curb). This width typically accommodates a single travel lane in each direction, a continuous center left-turn lane, and bike lanes.

**Residential Collector Policy:** District policy 7206.5.2 states that the standard street section for a collector in a residential area shall be 36-feet (back-of-curb to back-of-curb). The District will consider a 33-foot or 29-foot street section with written fire department approval and taking into consideration the needs of the adjacent land use, the projected volumes, the need for bicycle lanes, and on-street parking.

**Half Street Policy:** District Policy 7206.2.2 required improvements shall consist of pavement widening to one-half the required width, including curb, gutter and concrete sidewalk (minimum 5-feet), plus 12-feet of additional pavement widening beyond the centerline established for the street to provide an adequate roadway surface, with the pavement crowned at the ultimate centerline. A 3-foot wide gravel shoulder and a borrow ditch sized to accommodate the roadway storm runoff shall be constructed on the unimproved side.

**Sidewalk Policy:** District policy 7206.5.6 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all collector streets. A parkway strip at least 6-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip. Sidewalks constructed next to the back-of-curb shall be a minimum of 7-feet wide.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**ACHD Master Street Map:** ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, collector street requirements, and specific roadway features required through development. This segment of Moon Valley Road is designated in the MSM as a Town Center Collector with 2-lanes and on-street bike lanes, a 36-foot street section within 54-feet of right-of-way.

- c. **Applicant Proposal:** The applicant is proposing to construct Moon Valley Road, abutting the site as ½ of a 36-foot street section to include pavement widening, vertical curb, gutter and a 7-

foot wide planter strip within 25-feet of right-of-way and 5-foot wide detached concrete sidewalk located outside of the dedicated right-of-way abutting the site.

- d. **Staff Comments/Recommendations:** The applicant's proposal meets District policy and should be approved, as proposed.

Provide a permanent easement to 2-feet behind back of sidewalk for any sidewalk located outside of the dedicated right-of-way.

As noted above, the existing right-of-way along Moon Valley Road abutting the site is prescriptive right-of-way. Therefore, the applicant should be required to dedicate right-of-way from centerline of Moon Valley Road to extend to 2-feet behind back of curb abutting the site.

### 3. Herons Flight Lane

- a. **Existing Conditions:** Herons Flight Lane terminates in a cul-de-sac turnaround at the site's northwest property line and is improved with 2-travel lanes, and no curb, gutter or sidewalk abutting the site. There is 62-feet of right-of-way for Herons Flight Lane abutting the site (35-feet from centerline) with an additional 128-feet of right-of-way located at the terminus of the roadway to allow for a cul-de-sac turnaround. The cul-de-sac turnaround is partially constructed at the terminus of the roadway.

Herons Flight Lane continues as a 25-foot wide private roadway from the cul-de-sac turnaround to the south.

- b. **Policy:**

**Local Roadway Policy:** District Policy 7207.2.1 states that the developer is responsible for improving all local street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

**Street Section and Right-of-Way Policy:** District Policy 7207.5 states that right-of-way widths for all local streets shall generally not be less than 47-feet wide and that the standard street section shall be 33-feet (back-of-curb to back-of-curb).

**Standard Urban Local Street—33-foot Street Section and Right-of-way Policy:** District Policy 7207.5.2 states that the standard street section shall be 33-feet (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size. This street section shall include curb, gutter, and minimum 5-foot wide concrete sidewalks on both sides and shall typically be constructed within 47-feet of right-of-way.

For the City of Kuna and City of Star: Unless otherwise approved by Kuna or Star, the standard street section shall be 36-feet (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size. This street section shall include curb, gutter, and minimum 5-foot wide concrete sidewalks on both sides and shall typically be constructed within 50-feet of right-of-way.

**Sidewalk Policy:** District Policy 7207.5.7 states that five-foot wide concrete sidewalk is required on both sides of all local street, except those in rural developments with net densities of one dwelling unit per 1.0 acre or less, or in hillside conditions where there is no direct lot frontage, in which case a sidewalk shall be constructed along one side of the street. Some local jurisdictions may require wider sidewalks.

The sidewalk may be placed next to the back-of-curb. Where feasible, a parkway strip at least 8-feet wide between the back-of-curb and the street edge of the sidewalk is recommended to provide increased safety and protection of pedestrians and to allow for the planting of trees in accordance with the District's Tree Planting Policy. If no trees are to be planted in the parkway strip, the applicant may submit a request to the District, with justification, to reduce the width of the parkway strip.

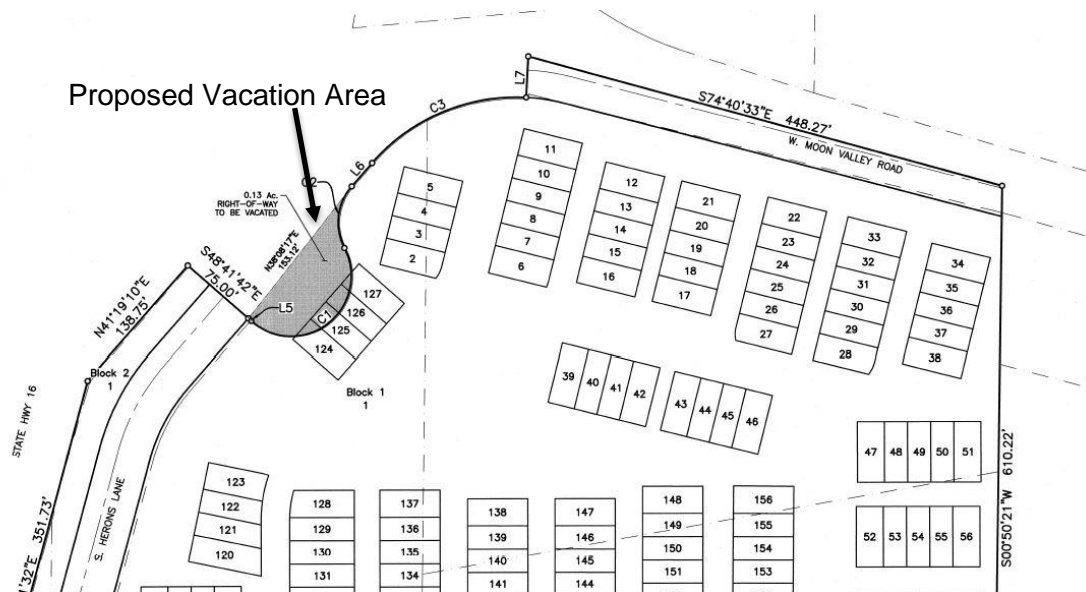
Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**Half Street Policy:** District Policy 7207.2.2 required improvements shall consist of pavement widening to one-half the required width, including curb, gutter and concrete sidewalk (minimum 5-feet), plus 12-feet of additional pavement widening beyond the centerline established for the street to provide an adequate roadway surface, with the pavement crowned at the ultimate centerline. A 3-foot wide gravel shoulder and a borrow ditch sized to accommodate the roadway storm runoff shall be constructed on the unimproved side.

- c. **Applicant's Proposal:** The applicant is proposing to improve Herons Flight Lane with curb, gutter and 5-foot wide attached concrete sidewalk abutting the site.

The applicant is proposing to vacate a portion of the cul-de-sac turnaround at the terminus of the public portion of Herons Flight Lane abutting the site and is proposing permanent structures within the proposed vacation area.



- d. **Staff Comments/Recommendations:** Consistent with ACHD's action in 2020 for Moon Valley Commons, the applicant should be required to improve Herons Flight Lane as ½ of a 36-foot wide local street section to include pavement widening, curb, gutter and 5-foot wide concrete sidewalk abutting the site.

The applicant's proposal to vacate a portion of the cul-de-sac turnaround at the terminus of the public portion of Herons Flight Lane does not meet District policy which requires that cul-de-sac turnarounds to be constructed at the terminus of stub and dead-end streets that extend greater than 150-feet, as they provide a place for the public, delivery and service vehicles, and ACHD maintenance vehicles to turn around; this segment of Herons Flight Lane is approximately 290-feet in length and should not be approved as proposed.

If the applicant chooses to apply to vacate a portion of Herons Flight Lane, the vacation requires a separate application and has its own approval process. Additionally, as part of the vacation application, the construction of a cul-de-sac turnaround would be recommended by staff, per policy, at the terminus of the public roadway. The cul-de-sac turnaround should be constructed

with curb, gutter, and a 5-foot wide attached sidewalk and with a minimum radius of 50-feet. The vacation should be completed prior to plan approval and ACHD's signature on the final plat.

The applicant should be required to revise the preliminary plat to remove the permanent structures within the proposed vacation area prior to plan submittal.

The applicant should be required to improve the cul-de-sac turnaround at the terminus of Herons Flight Lane with curb, gutter and 5-foot wide concrete sidewalk.

Dedicate additional right-of-way to total 2-feet behind of the back of sidewalk or provide a permanent easement to 2-feet behind back of sidewalk for any sidewalk located outside of the dedicated right-of-way on Herons Flight Way abutting the site.

## 4. Private Roads

- a. **Existing Conditions:** Herons Flight Lane exists as a 25-foot wide private road extending south from the cul-de-sac terminus of the public portion of Herons Flight Lane.
- b. **Private Road Policy:** District policy 7212.1 states that the lead land use agencies in Ada County establish the requirements for private streets. The District retains authority and will review the proposed intersection of a private and public street for compliance with District intersection policies and standards. The private road should have the following requirements:
  - Designed to discourage through traffic between two public streets,
  - Graded to drain away from the public street intersection, and
  - If a private road is gated, the gate or keypad (if applicable) shall be located a minimum of 50-feet from the near edge of the intersection and a turnaround shall be provided.
- c. **Applicant Proposal:** The applicant is proposing to utilize the existing private portion of Herons Flight Lane to provide access to the site. All other roadways within the site are proposed to be constructed as private roads.
- d. **Staff Comments/Recommendations:** If the City of Star approves the private road, and if Herons Flight Lane is widened, then the applicant shall be required to pave the private roadways their full widths and at least 30-feet into the site beyond the edge of pavement of all public streets and install pavement tapers with 15-foot curb radii abutting the existing roadway edge. If private roads are not approved by the City of Star, the applicant will be required to revise and resubmit the preliminary plat to provide public standard local streets in these locations.

Street name and stop signs are required for the private road. The signs may be ordered through the District. Verification of the correct, approved name of the road is required.

ACHD does not make any assurances that the private road, which is a part of this application, will be accepted as a public road if such a request is made in the future. Substantial redesign and reconstruction costs may be necessary in order to qualify this road for public ownership and maintenance.

The following requirements must be met if the applicant wishes to dedicate the roadway to ACHD:

- Dedicate a minimum of 50-feet of right-of-way for the road.
- Construct the roadway to the minimum ACHD requirements.
- Construct a stub street to the surrounding parcels.

## 5. Driveways

### 5.1 Moon Valley Road

- a. **Existing Conditions:** There is an existing driveway from the site onto Moon Valley Road located 274-feet west of the site's east property line.
- b. **Policy:**



**Access Policy:** District Policy 7205.4.1 states that all access points associated with development applications shall be determined in accordance with the policies in this section and Section 7202. Access points shall be reviewed only for a development application that is being considered by the lead land use agency. Approved access points may be relocated and/or restricted in the future if the land use intensifies, changes, or the property redevelops.

District Policy 7206.1 states that the primary function of a collector is to intercept traffic from the local street system and carry that traffic to the nearest arterial. A secondary function is to service adjacent property. Access will be limited or controlled. Collectors may also be designated at bicycle and bus routes.

**Driveway Location Policy (Stop Controlled Intersection):** District policy 7206.4.4 requires driveways located on collector roadways near a STOP controlled intersection to be located outside of the area of influence; OR a minimum of 150-feet from the intersection, whichever is greater. Dimensions shall be measured from the centerline of the intersection to the centerline of the driveway.

**Successive Driveways:** District policy 7206.4.5 Table 1, requires driveways located on collector roadways with a speed limit of 35 MPH and daily traffic volumes greater than 100 VTD to align or offset a minimum of 285-feet from any existing or proposed driveway.

**Driveway Width Policy:** District policy 7206.4.6 restricts high-volume driveways (100 VTD or more) to a maximum width of 36-feet and low-volume driveways (less than 100 VTD) to a maximum width of 30-feet. Curb return type driveways with 30-foot radii will be required for high-volume driveways with 100 VTD or more. Curb return type driveways with 15-foot radii will be required for low-volume driveways with less than 100 VTD.

**Driveway Paving Policy:** Graveled driveways abutting public streets create maintenance problems due to gravel being tracked onto the roadway. In accordance with District policy, 7206.4.6, the applicant should be required to pave the driveway its full width and at least 30-feet into the site beyond the edge of pavement of the roadway and install pavement tapers in accordance with Table 2 under District Policy 7206.4.6.

**Minor Improvements Policy:** District Policy 7203.3 states that minor improvements to existing streets adjacent to a proposed development may be required. These improvements are to correct deficiencies or replace deteriorated facilities. Included are sidewalk construction or replacement; curb and gutter construction or replacement; replacement of unused driveways with curb, gutter and sidewalk; installation or reconstruction of pedestrian ramps; pavement repairs; signs; traffic control devices; and other similar items.

- c. **Applicant's Proposal:** The applicant is proposing to close the existing driveway onto Moon Valley Road located 274-feet west of the site's east property line with vertical curb, gutter, a 7-foot wide landscape strip and 5-foot wide detached concrete sidewalk.
- d. **Staff Comments/Recommendations:** The applicant's proposal to close the existing driveway on Moon Valley Road meets District policy and should be approved as proposed.

## 6. Tree Planters

**Tree Planter Policy:** Tree Planter Policy: The District's Tree Planter Policy prohibits all trees in planters less than 8-feet in width without the installation of root barriers. Class II trees may be allowed in planters with a minimum width of 8-feet, and Class I and Class III trees may be allowed in planters with a minimum width of 10-feet.

## 7. Landscaping

**Landscaping Policy:** A license agreement is required for all landscaping proposed within ACHD right-of-way or easement areas. Trees shall be located no closer than 10-feet from all public storm drain facilities. Landscaping should be designed to eliminate site obstructions in the vision triangle

at intersections. District Policy 5104.3.1 requires a 40-foot vision triangle and a 3-foot height restriction on all landscaping located at an uncontrolled intersection and a 50-foot offset from stop signs. Landscape plans are required with the submittal of civil plans and must meet all District requirements prior to signature of the final plat and/or approval of the civil plans.

## **8. Other Access**

Moon Valley Road is classified as a collector roadway. Direct lot access is prohibited to this roadway and should be noted on the final plat.

## **D. Site Specific Conditions of Approval**

1. Revise the preliminary plat to remove the permanent structures shown within the proposed vacation area on Herons Flight Lane prior to plan submittal.
2. Construct Moon Valley Road, abutting the site as ½ of a 36-foot street section to include pavement widening, vertical curb, gutter and a 7-foot wide planter strip within 25-feet of right-of-way and 5-foot wide detached concrete sidewalk located outside of the dedicated right-of-way abutting the site. If street trees are desired, then provide an 8-foot wide planter strip. Provide a permanent easement to 2-feet behind back of sidewalk for any sidewalk located outside of the dedicated right-of-way.
3. Dedicate the existing prescriptive right-of-way abutting the site from centerline of Moon Valley Road to extend to 2-feet behind back of curb.
4. Improve Herons Flight Lane as ½ of a 36-foot wide local street section to include pavement widening, curb, gutter and 5-foot wide concrete sidewalk abutting the site.
5. Improve the cul-de-sac turnaround at the terminus of Herons Flight Lane with curb, gutter and 5-foot wide concrete sidewalk.
6. Dedicate additional right-of-way to total 2-feet behind of the back of sidewalk or provide a permanent easement to 2-feet behind back of sidewalk for any sidewalk located outside of the dedicated right-of-way on Herons Flight Way abutting the site.
7. Utilize the existing private roadway located at the terminus of Herons Flight Lane to provide access to the site and if widened, pave the private roadway its full width and at least 30-feet into the site. If a gate(s) or keypad is proposed for the private road, they shall be located a minimum of 50-feet from the near edge of the intersection.
8. A street name and stop sign are required for the private road if there is not one in place. The signs may be ordered through the District. Verification of the correct approved name of the road is required.
9. Close the existing driveway from the site onto Moon Valley Road located 274-feet west of the site's east property line with vertical curb, gutter, a 7-foot wide landscape strip and 5-foot wide detached concrete sidewalk, as proposed.
10. Direct lot access is prohibited to Moon Valley Road and should be noted on the final plat.
11. Submit civil plans to ACHD Development Services for review and approval. The impact fee assessment will not be released until the civil plans are approved by ACHD.
12. Payment of impact fees is due prior to issuance of a building permit.
13. Comply with all Standard Conditions of Approval.

## **E. Standard Conditions of Approval**

1. All proposed irrigation facilities shall be located outside of the ACHD right-of-way (including all easements). Any existing irrigation facilities shall be relocated outside of the ACHD right-of-way (including all easements).
2. Private Utilities including sewer or water systems are prohibited from being located within the ACHD right-of-way.
3. In accordance with District policy, 7203.3, the applicant may be required to update any existing non-compliant pedestrian improvements abutting the site to meet current Americans with Disabilities Act (ADA) requirements. The applicant's engineer should provide documentation of ADA compliance to District Development Review staff for review.
4. Replace any existing damaged curb, gutter and sidewalk and any that may be damaged during the construction of the proposed development. Contact Construction Services at 387-6280 (with file number) for details.
5. A license agreement and compliance with the District's Tree Planter policy is required for all landscaping proposed within ACHD right-of-way or easement areas.
6. All utility relocation costs associated with improving street frontages abutting the site shall be borne by the developer.
7. It is the responsibility of the applicant to verify all existing utilities within the right-of-way. The applicant at no cost to ACHD shall repair existing utilities damaged by the applicant. The applicant shall be required to call DIGLINE (1-811-342-1585) at least two full business days prior to breaking ground within ACHD right-of-way. The applicant shall contact ACHD Traffic Operations 387-6190 in the event any ACHD conduits (spare or filled) are compromised during any phase of construction.
8. Utility street cuts in pavement less than five years old are not allowed unless approved in writing by the District. Contact the District's Utility Coordinator at 387-6258 (with file numbers) for details.
9. All design and construction shall be in accordance with the ACHD Policy Manual, ISPWC Standards and approved supplements, Construction Services procedures and all applicable ACHD Standards unless specifically waived herein. An engineer registered in the State of Idaho shall prepare and certify all improvement plans.
10. Construction, use and property development shall be in conformance with all applicable requirements of ACHD prior to District approval for occupancy.
11. No change in the terms and conditions of this approval shall be valid unless they are in writing and signed by the applicant or the applicant's authorized representative and an authorized representative of ACHD. The burden shall be upon the applicant to obtain written confirmation of any change from ACHD.
12. If the site plan or use should change in the future, ACHD Planning Review will review the site plan and may require additional improvements to the transportation system at that time. Any change in the planned use of the property which is the subject of this application, shall require the applicant to comply with ACHD Policy and Standard Conditions of Approval in place at that time unless a waiver/variance of the requirements or other legal relief is granted by the ACHD Commission.

## **F. Conclusions of Law**

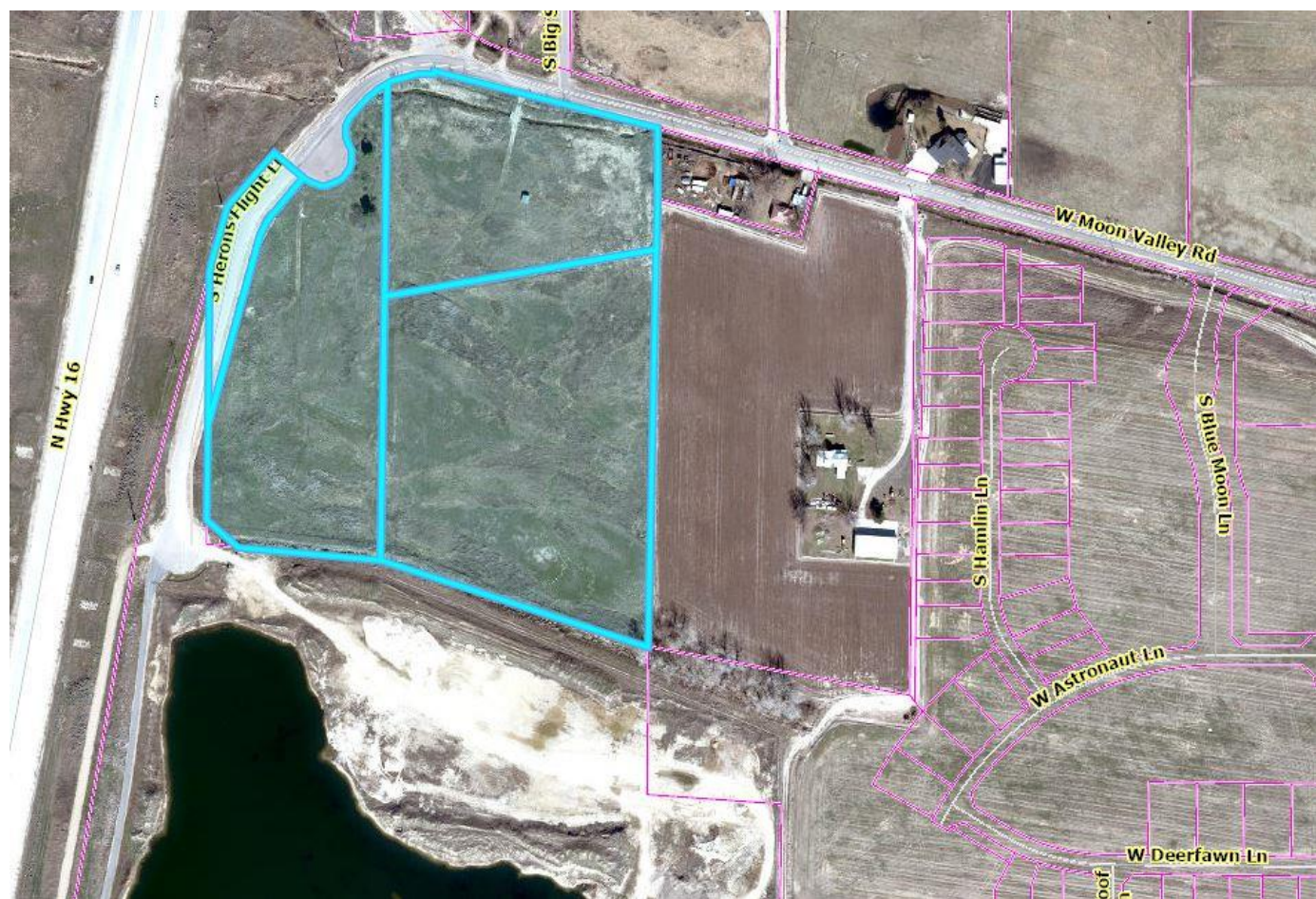
1. The proposed site plan is approved, if all of the Site Specific and Standard Conditions of Approval are satisfied.
2. ACHD requirements are intended to assure that the proposed use/development will not place an undue burden on the existing vehicular transportation system within the vicinity impacted by the proposed development.

## **G. Attachments**

1. Vicinity Map
2. Site Plan
3. TIS
4. ITD Memo
5. Utility Coordinating Council
6. Development Process Checklist
7. Appeal Guidelines



VICINITY MAP



# SITE PLAN



# Ada County Utility Coordinating Council

## Developer/Local Improvement District Right of Way Improvements Guideline Request

*Purpose: To develop the necessary avenue for proper notification to utilities of local highway and road improvements, to help the utilities in budgeting and to clarify the already existing process.*

- 1) **Notification:** Within five (5) working days upon notification of required right of way improvements by Highway entities, developers shall provide written notification to the affected utility owners and the Ada County Utility Coordinating Council (UCC). Notification shall include but not be limited to, project limits, scope of roadway improvements/project, anticipated construction dates, and any portions critical to the right of way improvements and coordination of utilities.
- 2) **Plan Review:** The developer shall provide the highway entities and all utility owners with preliminary project plans and schedule a plan review conference. Depending on the scale of utility improvements, a plan review conference may not be necessary, as determined by the utility owners. Conference notification shall also be sent to the UCC. During the review meeting the developer shall notify utilities of the status of right of way/easement acquisition necessary for their project. At the plan review conference each company shall have the right to appeal, adjust and/or negotiate with the developer on its own behalf. Each utility shall provide the developer with a letter of review indicating the costs and time required for relocation of its facilities. Said letter of review is to be provided within thirty calendar days after the date of the plan review conference.
- 3) **Revisions:** The developer is responsible to provide utilities with any revisions to preliminary plans. Utilities may request an updated plan review meeting if revisions are made in the preliminary plans which affect the utility relocation requirements. Utilities shall have thirty days after receiving the revisions to review and comment thereon.
- 4) **Final Notification:** The developer will provide highway entities, utility owners and the UCC with final notification of its intent to proceed with right of way improvements and include the anticipated date work will commence. This notification shall indicate that the work to be performed shall be pursuant to final approved plans by the highway entity. The developer shall schedule a preconstruction meeting prior to right of way improvements. Utility relocation activity shall be completed within the times established during the preconstruction meeting, unless otherwise agreed upon.

**Notification to the Ada County UCC can be sent to:** 50 S. Cole Rd. Boise 83707, or Visit [iducc.com](http://iducc.com) for e-mail notification information.



# Development Process Checklist

## Items Completed to Date:

- ☒ Submit a development application to a City or to Ada County
- ☒ The City or the County will transmit the development application to ACHD
- ☒ The ACHD **Planning Review Section** will receive the development application to review
- ☒ The **Planning Review Section** will do one of the following:
  - ☐ Send a **"No Review"** letter to the applicant stating that there are no site specific conditions of approval at this time.
  - ☒ Write a **Staff Level** report analyzing the impacts of the development on the transportation system and evaluating the proposal for its conformance to District Policy.
  - ☒ Write a **Commission Level** report analyzing the impacts of the development on the transportation system and evaluating the proposal for its conformance to District Policy.

## Items to be completed by Applicant:

- ☐ For **ALL** development applications, including those receiving a **"No Review"** letter:
  - The applicant should submit one set of engineered plans directly to ACHD for review by the **Development Review Section** for plan review and assessment of impact fees. (Note: if there are no site improvements required by ACHD, then architectural plans may be submitted for purposes of impact fee assessment.)
  - The applicant is required to get a permit from Construction Services (ACHD) for ANY work in the right-of-way, including, but not limited to, driveway approaches, street improvements and utility cuts.
- ☐ Pay Impact Fees prior to issuance of building permit. Impact fees cannot be paid prior to plan review approval.

### DID YOU REMEMBER:

#### *Construction (Non-Subdivisions)*

- ☐ **Driveway or Property Approach(s)**
  - Submit a "Driveway Approach Request" form to ACHD Construction (for approval by Development Services & Traffic Services). There is a one week turnaround for this approval.
- ☐ **Working in the ACHD Right-of-Way**
  - Four business days prior to starting work have a bonded contractor submit a "Temporary Highway Use Permit Application" to ACHD Construction – Permits along with:
    - a) Traffic Control Plan
    - b) An Erosion & Sediment Control Narrative & Plat, done by a Certified Plan Designer, if trench is >50' or you are placing >600 sf of concrete or asphalt.

#### *Construction (Subdivisions)*

- ☐ **Sediment & Erosion Submittal**
  - At least one week prior to setting up a Pre-Construction Meeting an Erosion & Sediment Control Narrative & Plan, done by a Certified Plan Designer, must be turned into ACHD Construction to be reviewed and approved by the ACHD Stormwater Section.
- ☐ **Idaho Power Company**
  - Vic Steelman at Idaho Power must have his IPCO approved set of subdivision utility plans prior to Pre-Con being scheduled.
- ☐ **Final Approval from Development Services is required** prior to scheduling a Pre-Con.

## Request for Appeal of Staff Decision

1. **Appeal of Staff Decision:** The Commission shall hear and decide appeals by an applicant of the final decision made by the Development Services Manager when it is alleged that the Development Services Manager did not properly apply this section 7101.6, did not consider all of the relevant facts presented, made an error of fact or law, abused discretion or acted arbitrarily and capriciously in the interpretation or enforcement of the ACHD Policy Manual.
  - a. **Filing Fee:** The Commission may, from time to time, set reasonable fees to be charged the applicant for the processing of appeals, to cover administrative costs.
  - b. **Initiation:** An appeal is initiated by the filing of a written notice of appeal with the Secretary and Clerk of the District, which must be filed within ten (10) working days from the date of the decision that is the subject of the appeal. The notice of appeal shall refer to the decision being appealed, identify the appellant by name, address and telephone number and state the grounds for the appeal. The grounds shall include a written summary of the provisions of the policy relevant to the appeal and/or the facts and law relied upon and shall include a written argument in support of the appeal. The Commission shall not consider a notice of appeal that does not comply with the provisions of this subsection.
  - c. **Time to Reply:** The Development Services Manager shall have ten (10) working days from the date of the filing of the notice of appeal to reply to the notice of the appeal, and may during such time meet with the appellant to discuss the matter, and may also consider and/or modify the decision that is being appealed. A copy of the reply and any modifications to the decision being appealed will be provided to the appellant prior to the Commission hearing on the appeal.
  - d. **Notice of Hearing:** Unless otherwise agreed to by the appellant, the hearing of the appeal will be noticed and scheduled on the Commission agenda at a regular meeting to be held within thirty (30) days following the delivery to the appellant of the Development Services Manager's reply to the notice of appeal. A copy of the decision being appealed, the notice of appeal and the reply shall be delivered to the Commission at least one (1) week prior to the hearing.
  - e. **Action by Commission:** Following the hearing, the Commission shall either affirm or reverse, in whole or part, or otherwise modify, amend or supplement the decision being appealed, as such action is adequately supported by the law and evidence presented at the hearing.



Chris Lombardo  
189 S. Moon Beam Way  
Eagle, Idaho 83616

August 30, 2021

Shawn Nickel  
Star Planning & Zoning  
Star, Idaho 83669

Dear Shawn,

It has come to my attention that there is a subdivision planned for 156 Town Homes to be built at the west end of Moon Vally Road. Please consider this letter as an opposing view to this type of development. This entire area is more of a rural area with space between homes, horses, and an area for kids to play and enjoy nice walks. The very last thing we need is a high-density project that will only bring renters and landlords to the area. We must maintain what the area is and not simply more rooftops to collect property taxes. In addition to maintaining the current characteristics of the neighborhood we must consider the local traffic. 156 townhomes will bring an additional 300 cars per day down Palmer Lane, Short Road and Moon Beam Road. It is simply dangerous at this point to try and get onto Hwy 44 in the mornings and back home in the evenings. The City of Star needs to consider those who already live in this area and not simply what one more development does of the city. Until there is something done with Hwy 44 no further development should be considered. I strongly oppose this type of development and so should the City of Star.

Sincerely,



Chris Lombardo

## Shawn Nickel

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**From:** Betsy Dudley <edudley9815@gmail.com>  
**Sent:** Tuesday, August 31, 2021 11:53 AM  
**To:** Shawn Nickel  
**Subject:** Proposed addition to Riverstone subdivision

To the Star City Council:

I am a resident of 6615 Moon Valley Rd., Eagle, ID 83616. Since I have lived here (roughly 16 years), three substantial subdivisions have been—or are being—built. For a country road that is only 1 1/2 miles long, adding 300 homes is overwhelming us with traffic and noise. Now apparently there is an application from Riverstone for 156 MORE townhomes (rentals, I understand) at our west end. This is in addition to the already-approved 268 homes there. We also cannot forget the overload of churchgoers who (even with two ways to access State Street from church property) speed east down Moon Valley Rd. every Sunday morning.

While this application is separate from its counterpart at the east end of the road, the fact that there are 60 approved lots going in there should be considered when thinking about traffic congestion. Most of the houses on each end have not yet been built and therefore the true numbers of cars, trucks and motorcycles cannot be accurately measured. However, the national average of 10 vehicle trips per household per day, 1424 just from Riverstone alone, is unimaginable. I understand also that some of the older subdivisions still have lots for homes not yet built.

While I understand that domestic water and sewage will be handled by the City of Star, will previously agricultural water rights be used for domestic lawns, gardens, and public spaces? This year, our irrigation water was turned off in August, two months earlier than usual.

In consideration of all of these issues, I am adamantly opposed to the Star Council approving this application.

Sincerely,  
Elizabeth (Betsy) Dudley

Sent from my iPad

Betsy Dudley

## Shawn Nickel

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**From:** Carmen Little <carmenlittle@cableone.net>  
**Sent:** Thursday, August 26, 2021 4:26 PM  
**To:** Shawn Nickel  
**Subject:** Development on Moon Valley Rd.

Good Afternoon Mr. Nickel,

I'm writing in opposition to the development of 156 town homes at the end of Moon Valley Road and The Fountain Park Subdivision.

We are recent residents in Riverstone (I have lived in Boise all of my 57 years), and have only been accessing highway 44 for 3 weeks. The traffic is busy all day long and it is very difficult to make a left turn out of both Short Lane and Palmer Lane. As of now, I believe there are under 50 homes occupied in Riverstone out of the 256 approved – imagine the traffic when Riverstone is completed. The Eagle Christian Church has already noticed residents/visitors cutting through their parking lot and have put up signs asking people not to cut through. Continued development by the cities of Star and Eagle along highway 16, highway 44, Palmer Lane, Short Lane, etc., will obviously continue to add car trips to the already congested two lane highways. We all know it will just be a matter of time (probably less than 1 year) until there will be a fatality at one of the intersections mentioned above as people take risks more and more often to access the roadways.

PLEASE take some time to let the highway infrastructure develop before continued high density residential properties are approved. Before long Star will be another casualty like Eagle and Meridian, losing it's small town feel, close knit neighbors and it's ability to provide services for a town that is overpopulated.

Sincerely,

Carmen Little  
314 S. Hamlin Ln.  
Eagle, ID 83616

## Shawn Nickel

---

**From:** jan griffith <jan.griffith@yahoo.com>  
**Sent:** Monday, August 30, 2021 5:16 PM  
**To:** Shawn Nickel  
**Cc:** sullivan.hal@gmail.com  
**Subject:** Riverstone Rental Townhomes Moon Valley

Dear Shawn,

Please put us on record as opposing Riverstone Rental Townhomes to be located at the west end of Moon Valley Rd.

An access other than Moon Valley Road would need to be in place prior to any consideration of said application or approval due to the extreme traffic said application would create on Moon Valley Rd.

Wyllie and Jan Griffith  
7010 W Moon Valley Rd  
Eagle, Idaho 83616

RECEIVED  
AUG 3 11 2021  
BN

**MOON VALLEY ROAD DEVELOPMENT**

SEAN NICKEL, CITY OF STAR PLANNER

Dear Neighbors,

We all treasure the serenity of our rural community serviced by a very narrow two lane country road. Nice for kids, bikes, horses and walks.

Many of us have seen the impact from recent building on Moon Valley Road, long waits to get on State Highway 44 (some residents say as long as 20 minutes) as well as heavy traffic on Moon Valley Road. Eagle Christian Church has posted a sign asking people not to use their parking lot as access to Hwy 44.

We have a beautiful community, consisting of larger acreage, farms and ranches. But, high density development threatens. The developer of Riverstone (single family homes) submitted to the city of Star an application to approve an additional 156 attached (rental) townhomes at the west end of Moon Valley Road.

Keep in mind, the National Average of trips per day per household is 10. Riverstone has 268 homes approved (only 50 occupied), plus the additional 156 townhomes equals 4240 car trips per day. Even if you halved that number Moon Valley Road will soon be a congested mess.

Please, take a moment and write a letter opposing the Density as well as getting the developer in tune with the rural character we love. Don't let greed and profit for the investor developer take away what brought us here.

Additionally, Stopping large scale projects until traffic on 44 is addressed is also a major concern. Please Address all correspondence to:

Shawn Nickel

Planning Director

10769 W State Street

Star, Idaho 83669

If you prefer, I will accept your letter and personally hand deliver them. All objections must be received in hand by August 30<sup>th</sup>. There will be a public hearing sometime around September 7<sup>th</sup>.

Hal and Carolyn Sullivan 757 South Moon Beam Way, Eagle 83616 [sullivan.hal@gmail.com](mailto:sullivan.hal@gmail.com)  
949.306.6022 Cell

Thank You

Hal Sullivan  
Carolyn Sullivan



H<sup>BAR</sup>H VETERINARY CLINIC  
**STEPHEN HARDY, DVM**  
P.O. Box 603  
Eagle, Idaho 83616  
Phone (208) 286-7828

RECEIVED  
AUG 3 0 2021  
BY: BN

August 24, 2021

Shawn Nickel  
Planning Director  
10769 W State Street  
Star ID 83669

Dear Mr. Nickel:

We are writing this letter to express our strong opposition to the high density growth we are experiencing on Moon Valley Road. Proper planning *should* involve a good dose of common sense - but in the current times, it seems common sense has been thrown to the wind. As long time residents (1982) and rural business owners, we have seen this community over run by development already as far as infrastructure is concerned. The existing traffic is untenable, this is obvious. It seems that greed must be involved, for how else do you explain the lunacy of granting further high density housing?

We, as Moon Valley Road residents, have previously been lied to at public hearings in Star about the quantity of truck traffic and amounts of gravel hauled to and fro on our tiny little road. The one and only stoplight was removed to satisfy the "rules" for accepting federal money for the Hwy 16 connector. It is also ludicrous that over development is rubber stamped and the fact that we live in the area of impact but are unable to vote the perpetrators in our out of office.

There is no good reason for granting further development on this road. Please try and preserve some semblance of our rural life style by stopping this massive overdevelopment. It would be best if efforts were made to handle our current influx with much needed infrastructure. Not to mention that we are going to come up short on natural resources like water without proper planning.

Hopefully at this public meeting there won't be a pre-determined decision made "behind closed doors" and that you will truly listen and act on the concerns of the residents of this area, as we know our neighbors feel the same. Thank you for your time in hearing these concerns.

Sincerely,

  
Steve & Dani Hardy

**Jeffrey S. and Christal V. Taylor**  
**6657 W. Moon Valley Rd.**  
**Eagle, Idaho 83616**

RECEIVED  
AUG 3 0 2021  
BY: BN

**Contact**

208-794-3036  
Jeffreys.taylor@icloud.com

Shawn Nickel  
Planning Director  
City of Star  
10769 W. State Street  
Star, ID 83669


**Dear Shawn,**

The purpose of the letter is to express significant opposition to the application associated with the addition of 156 attached (rentals) townhomes at the west end of West Moon Valley Rd.

It's my understanding that 268 homes have already been approved in the Riverstone development. Given that congestion is already occurring with less than 25% occupancy, I can't imagine the congestion once fully complete. To approve further development, absent completed road improvements, presumably supported by traffic studies and neighborhood input, seems irresponsible and at a minimum, unsafe. There have already been numerous accidents at the intersection of Palmer Ln. and State St. and I've personally witnessed numerous near misses at the intersection of Moon Valley Rd. and State St.

For that reason, we respectfully ask that approval of further development be curtailed until these matters can be addressed.

Respectfully,



Jeffrey S. Taylor

Shawn Nickel  
Planning Director  
10769 West State Street  
SPR, IDAHO 83669

8-23-2021

RECEIVED  
AUG 30 2021  
BY: BN

Hi Shawn,  
Remember me Pete Cintonio (Clark  
Nursery & Landscape Contractors - Good friend of  
Mark Butler). Well Shawn it has been  
brought to my attention that there is a new  
Multi Family (Apartment) complex proposed for  
the end of Moon Valley Road by the developer  
of RiverStone sub division. I believe it is  
for 156 attached homes.

Well I personally want to let you know that  
I am opposed to such a development. This  
area on Moon Valley Road when I bought back  
in 2001 was supposed to be rural with one  
home per 5 acres and now look what it has  
turned out to be.

The Rural Road off Moon Valley is now  
a major thorough fair not even properly handling  
the current traffic. Once the existing approved  
sub divisions are built out it will be a  
real mess. Even today I have had to wait  
more than 15 minutes to get on Hwy 14.



SHAWN Remember this is IDIOT Not L.A.  
And we should not let his developers from  
out of state influence on ~~District~~ District  
What happens here. They are only concerned  
about making \$ Not Life Style.

Our Idiot style of life is being  
ruined one day at a time by these greedy  
developers.

Please do not let this Application pass  
Our style of life is too important. There is  
no way the area can handle this type  
of dirtiness never mind the water ect.  
And smog All this development will demand.

Thank you for considering my  
interests.

Peter Cintore

756 South Moor Beam Way

Engle, CA. 93616

1-208-870 2026

PJCoutdoors@hotmail.com

RECEIVED  
AUG 30 2021  
BY: BN

August 26, 2021

Shawn Nickel  
Planning Director  
City of Star  
10769 W. State Street  
Star, Idaho  
83669

Dear Shawn:

We are writing to address our concerns regarding the proposed addition of 156 townhomes at the west end of Moon Valley Road, a small, two lane road south of State Highway 44 between Eagle and Star. As 10+ year residents of Moon Lake Ranch we have continued to see the development of the surrounding properties on Moon Valley Road and the complete lack of foresight into ingress and egress issues for traffic onto State Highway 44. The increased traffic from the current construction efforts and new residents has already created serious traffic safety issues and numerous accidents. We are adamantly opposed to this development and any additional developments until a thorough long-term plan is created to address all growth issues, specifically the impact of increased traffic and safety for residents.

Sincerely,

Kelly & Marie Hendrix  
652 S. Moon Beam Way  
Eagle, Idaho 83616



RECEIVED  
AUG 30 2021  
BY: *BN*

Barbara Saevig  
490 S. Hamlin Ln  
Eagle, ID 83616  
barbarabkpr@gmail.com

August 25, 2021

Shawn Nickel  
Planning Director  
10769 W. State Street  
Star, ID 83669

RE: STRONG objection to proposed 156 attached townhomes on west end of Moon Valley Rd

We just purchased in Riverstone to GET AWAY FROM CONGESTION. Now, a 156 townhome development is being planed within yards of our HOME????? ABOSULUTELY NOT!!!!

***We will go to any lengths necessary to fight this development.***

Don't allow this blight to be built on this sight. There will already be way too much congestion with the Highway 16 throughway/freeway when it is continued past the 44/State St.

This is an awful thought, realizing that someone(s) would allow this dense development so close to us.

THERE IS SO MUCH LAND AVAILABLE IN LESS CONGESTED AREAS to build on. Move the proposal to build elsewhere.

**ABSOLUTELY NOT IN OUR BACKYARD!**



Barbara Saevig

August 24, 2021

RECEIVED  
AUG 30 2021  
BY: BN

Dear Mr. Shawn Nickel,

Subject: Riverstone, 156 rental townhomes at the west end of Moon Valley Road.

Moon Valley Road in its current state (two lanes with ditches on both sides) is just too narrow to handle the increased traffic developed by the new current building projects.

1. Seventy nine (79) new homes on east end of Moon Valley Road
2. Riverstone 268 home development on west end of Moon Valley Road
3. Proposed Riverstone 156 rental townhomes
4. Current expanding private ~~businesses~~ (Vets, Weldings, etc.)

Along with the above we also have our current homes and their expansions.

Don't forget school bus traffic that is very difficult to unload children on a narrow strip of gravel next to a ditch.

We ~~cannot~~ handle the increased traffic congestion that would result from the Riverstone 156 rental townhomes. At present it is very difficult to get in or out of Moon Valley Road to highway 44 without taking calculated traffic risks.

All of the above will greatly increase life and death accidents.

Please say no to the Riverstone development 156 rental Townhome proposal.

Sincerely,

Richard Margaret Farnsworth  
6155 West Half Moon Lane

RECEIVED  
AUG 30 2021  
BY: bx.

August 25, 2021

To Shawn Nichol,

My name is Steve Saevig, we recently purchased a home in the Riverstone Subdivision, we had been looking around for quite a while. We picked Riverstone because it was in a rural area, a small road to get in and a couple of small roads to get access to highway 44.

I am opposing the additional 156 townhomes, Riverstone will have 256 homes when it is finished along with a pool, clubhouse, gym and a small lake we can use. I feel that 156 more townhomes will push it over the edge with traffic on the small roads. It would be like living next to Eagle Road with their traffic.

Thank You

Steve



Barbara Saevig  
490 S. Hamlin Ln  
Eagle, ID 83616  
barbarabkpr@gmail.com

August 25, 2021

Shawn Nickel  
Planning Director  
10769 W. State Street  
Star, ID 83669

RE: STRONG objection to proposed 156 attached townhomes on west end of Moon Valley Rd

We just purchased in Riverstone to GET AWAY FROM CONGESTION. Now, a 156 townhome development is being planed within yards of our HOME????? ABOSULUTELY NOT!!!!

*We will go to any lengths necessary to fight this development.*

Don't allow this blight to be built on this sight. There will already be way too much congestion with the Highway 16 throughway/freeway when it is continued past the 44/State St.

This is an awful thought, realizing that someone(s) would allow this dense development so close to us.

THERE IS SO MUCH LAND AVAILABLE IN LESS CONGESTED AREAS to build on. Move the proposal to build elsewhere.

**ABSOLUTELY NOT IN OUR BACKYARD!**

Barbara Saevig

RECEIVED  
AUG 30 2021

BY: BV

08/23/2021

Shawn Nickel, Planning Director  
10769 W. State Street  
Star, ID 83669

Dear Mr. Nickel,

It has come to my attention that the Riverstone development has plans to build 156 townhomes on the site. I'm estimating this will bring in an additional 200+ vehicles who will be vying to merge onto/cross Hwy 44 daily. As one of the original home owners at Riverstone, I already find it dangerous trying to merge onto/cross Hwy 44 since it's a main roadway and constantly packed during commute hours. I've said since moving here that a signal light is needed on Hwy 44 at S. Short Road or another cross street from W. Moon Valley Rd. Hwy 44 also needs to be expanded to two lanes in each direction. Several residential developments are popping up along Hwy 44 and the traffic congestion will only get worse and more dangerous. The roadway infrastructure needs to be addressed before these developments are completed, not after.

I am very much against the building of the townhomes on the Riverstone site for the following reasons:

- 1) Not only will the extra traffic add to the delays and safety issues; the townhome residents will utilize W. Moon Valley Rd which my wife and I use for our daily walks and bike rides. Our quiet and safe country road will become a thoroughfare for the extra traffic caused by the townhome residents.
- 2) Townhomes will bring down the home value in our development and the surrounding area.
- 3) Townhomes bring in an excessive number of renters who do not share the same pride of ownership as the home owners.
- 4) Townhomes, just like any high-density housing, are a magnet for crime (vehicle burglaries, etc.). I have almost 40 years in law enforcement and have seen this first hand.

Thank you,

Barry and Kathy Barner  
602 S. Short Lane  
Eagle, ID 83616  
408-691-1241  
[barrybarner@comcast.net](mailto:barrybarner@comcast.net)



RECEIVED  
AUG 30 2021

BY: BN

August 23, 2021

Shawn Nickel

Planning Director

City of Star

Mr. Nickel, my name is Doug McAllister and I live at 209 S Palmer Lane. My location puts me dead center in the middle of the traffic and congestion created by the development Silver Stone and the newly proposed developments at the west end of Moon Valley Road. Our "country" life style and open spaces are changing, and although we realize some change is inevitable, we would like to see it tempered with common sense and compassion.

I hope you will realize that added a townhome condo complex will impact the traffic in my area significantly, and I don't think Hwy 44 has the capability to handle it. In fact Hwy 44 can't handle the current traffic load. My wife and I drive to work every day, and some mornings and evenings we have a considerable wait before we can safely merge onto Hwy 44.

Single family homes attract good people and put much less strain on the infrastructure than multi-family townhomes do. I hope you will consider these points when making a decision regarding the use of the land in my area.

Sincerely,

Doug McAllister

209 S Palmer Lane

Eagle, Id 83616

RECEIVED  
AUG 30 2021  
BY: BV

----- Forwarded message -----

From: [jao.email@yahoo.com](mailto:jao.email@yahoo.com) <[jao.email@yahoo.com](mailto:jao.email@yahoo.com)>  
Date: Fri, Aug 27, 2021, 6:27 PM  
Subject: Development at end of Moon Valley near HWY 16  
To: [sullivan.hal@gmail.com](mailto:sullivan.hal@gmail.com) <[sullivan.hal@gmail.com](mailto:sullivan.hal@gmail.com)>

Hal, for you to hand deliver:

Shawn Nickel  
Planning Director  
10769 w state street  
Star, ID 83669

I am against the high density development plans for the property located at the end of moon valley rd near highway 16. Please consider this request to not approve high density housing in this area. I assume the developer has to be allowed to build something but high density should not be approved. I have lived on moon valley since 1993 and it has been a relatively quiet road one would expect in a neighborhood but with what has already been approved in housing along moon valley and with this new development it will reach the point of being very unlivable. I will be forced out to have any quality of life in the future as my home is just 25 feet off the road. You have the power to not recommend approval due to traffic concerns and tell the developer to go back and redesign for much lower density housing which is bad, but not nearly as bad from a traffic perspective.  
Thank you for your consideration.

Quentin Nesbitt  
7435 Moon Valley Rd  
Eagle, ID. 83616

RECEIVED  
AUG 30 2021  
BY: BW

August 29, 2021

Mr. Shawn Nickel  
Planning Director  
10769 W State Street  
Star, ID 83669

Dear Mr. Nickel,

I am writing to express my concerns regarding the high density application being submitted by m3 Companies for the property adjacent to the Riverstone Development, at the end of Moon Valley Road. While we all understand the challenges you face in your position to plan for healthy growth in Star, there is huge responsibility to maintain growth in a way that is safe for Star and its residents. As a resident that has recently moved into the Riverstone development, I can attest personally that the traffic for the entire area around Highways 16 and 44 cannot handle the additional traffic that 156 high density homes would bring. Not counting other housing developments with larger lots, Riverstone alone has yet to be completed and currently has around 50 of the planned 268 homes occupied. Once you combine the balance of 218 families moving into Riverstone and add potentially 156 more families to enter and exit Highway 44 on Short Road, that is a disaster waiting to happen. I am seriously concerned for the safety of our family of 6, as well as the many other families in this area.

While it may be difficult to restrict all additional building until roads and city services can handle more, I do ask that you oppose the addition of high density housing. It will only stress the area and endanger its residents, which will far outweigh any benefits brought by the project.

Best regards,



Daniel Fiorello

7850 W Deerfawn Lane  
Eagle, ID 83616

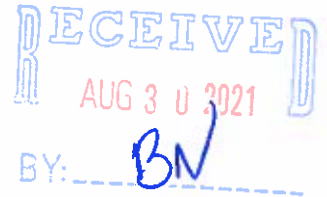
daniel@altanovamfg.com

Shawn Nickel

Planning Director

10769 W State St

Star, Id 83669



To Whom It May Concern:

We are residents of Riverstone, residing at 7833 and 7849 W Deerfawn Ln, Eagle. We wish to express our opinion about allowing further density in this area with the approval of the 156 townhomes that are proposed for the end of Moon Valley Rd. The neighborhood streets are not equipped to handle our resident traffic, much less additional traffic.

It is currently extremely difficult and dangerous attempting to get onto Highway 44. It takes so long to pull out, traffic gets backed up on the small access streets.

Please reconsider the approval to build these additional housing units.

Thank you,

Ed & Deanne Nonzeta

7833 W Deerfawn Ln

*Ed Nonzeta*  
*Deanne Nonzeta*

Lisa M. Jones

7849 W Deerfawn Ln

*Lisa M. Jones*

August 24<sup>th</sup> 2021

RECEIVED  
AUG 30 2021  
BY: BN

To whom it may concern:

I am writing this letter concerning the Moon Valley Development project. I live at 151 South Short Lane. I am a home owner and a small farm owner that would like to keep this area rural. We moved here 26 years ago to be out in the rural county area and raise cows and have a small farm. I have fought against the high growth in this area and also have gone to many council meetings and feel like we have not been heard. The high density and traffic that it causes is horrible. They have ruined our road on both ends of Short Rd due to all the construction, trucks and traffic. I am totally against adding 156 town houses at the end of Moon Valley Rd. Bringing 156 townhouses would increase the traffic that is already overloaded. Hwy 44 is already congested in the mornings and evenings. Don't let greed and money ruin are area. I am elderly and don't want the area to become even more congested. I worry about my farm and what all that growth would intel. Please here us and have the developer calm down the amount of houses that this area can obtain. This shouldn't be about how much money and profit that they can make. Once the land is gone are rural character is gone also.

Please here what we have to say.

Thank you



Helen M. Allen



Aug 25 2021

To Whom it may Concern :



I am very concerned about the Moon Valley Development going on in this area. I live at 153 So Short Lane. The traffic has become very heavy and the large trucks are tearing up this road and its getting much harder to get out onto Hwy 44.

Now the developer is planning another 156 rental townhomes which will allow much additional traffic. This has been a wonderful rural area great for walking, bikes, children at play, horses & other rural activities.

Please re-consider adding more Congestion at least until Hwy 44 can handle the large increase in traffic flow.

Thank you,  
Marilyn Fisk

I'm writing in opposition to the development of 156 town homes at the end of Moon Valley Road and The Fountain Park Subdivision.

RECEIVED  
AUG 3 0 2021  
BY: BV

We are recent residents in Riverstone (I have lived in Boise all of my 57 years), and have only been accessing highway 44 for 3 weeks. The traffic is busy all day long and it is very difficult to make a left turn out of both Short Lane and Palmer Lane. As of now, I believe there are under 50 homes occupied in Riverstone out of the 256 approved – imagine the traffic when Riverstone is completed. The Eagle Christian Church has already noticed residents/visitors cutting through their parking lot and have put up signs asking people not to cut through. Continued development by the cities of Star and Eagle along highway 16, highway 44, Palmer Lane, Short Lane, etc., will obviously continue to add car trips to the already congested two lane highways. We all know it will just be a matter of time (probably less than 1 year) until there will be a fatality at one of the intersections mentioned above as people take risks more and more often to access the roadways.

PLEASE take some time to let the highway infrastructure develop before continued high density residential properties are approved. Before long Star will be another casualty like Eagle and Meridian, losing it's small town feel, close knit neighbors and it's ability to provide services for a town that is overpopulated.

Sincerely,

Carmen Little

314 S. Hamlin Ln.

Eagle, ID 83616

----- Forwarded message -----

From: Shelli Richardson <[shellirichardson@me.com](mailto:shellirichardson@me.com)>

Date: Fri, Aug 27, 2021, 1:10 PM

Subject: Moon Valley Road Development

To: <[sullivan.hal@gmail.com](mailto:sullivan.hal@gmail.com)>

RECEIVED  
AUG 30 2021  
BY: BV

To Whom It May Concern,

I am a residence in the new Riverstone Community.

This letter is of concern for the traffic that is constantly increasing on the Moon Valley Rd. and the long waits to get out onto State Highway 44. I have experienced many times having to wait over 15 minutes to turn onto highway 44. This is very frustrating and is only going to increase as more residence move into Riverstone Community.

One of the reasons we moved to this community is the rural feeling and the quiet neighborhood. This is quickly changing and especially if 156 townhomes will be built. This is only going to increase the congestion in this area. This will also change the dynamics of this beautiful quiet neighborhood. I am very opposed to the density, safety and bottleneck this will cause.

I am opposing the development of 156 townhomes as being proposed. Please consider the detriment to this community if this is to be approved.

Thank you,

Shelli Richardson